



Highways Committee

Date Wednesday 25 September 2019
Time 9.30 a.m.
Venue Committee Room 2 - County Hall, Durham

Business

Part A

1. Apologies for Absence
2. Substitute Members
3. Minutes of the meetings held on 5 July and 2 September 2019 (Pages 3 - 24)
4. Declarations of Interest, if any
5. Coxhoe - Parking & Waiting Restrictions Order 2019 - Report of Corporate Director of Regeneration and Local Services (Pages 25 - 46)
6. Barnard Castle - Parking & Waiting Restrictions Order 2019 - Report of Corporate Director of Regeneration and Local Services (Pages 47 - 64)
7. Such other business, as in the opinion of the Chairman of the meeting, is of sufficient urgency to warrant consideration

Helen Lynch
Head of Legal and Democratic Services

County Hall
Durham
17 September 2019

To: **The Members of the Highways Committee**

Councillor C Kay (Chair)
Councillor S Morrison (Vice-Chair)

Councillors D Bell, H Bennett, G Bleasdale, J Chaplow,
J Considine, S Dunn, D Hicks, K Hopper, S Hugill, K Liddell,
O Milburn, R Ormerod, J Rowlandson, P Sexton,
J Shuttleworth, A Simpson, K Thompson, J Turnbull and
M Wilson

Contact: Michael Turnbull Tel: 03000 269 714

DURHAM COUNTY COUNCIL

At a Meeting of **Highways Committee** held in Committee Room 2 - County Hall, Durham on **Friday 5 July 2019 at 9.30 a.m.**

Present:

Councillor C Kay in the Chair

Members of the Committee

Councillors J Considine, O Gunn, D Hicks, K Hopper, I Jewell, K Liddell, O Milburn, S Morrison (Vice-Chair), A Reed, J Robinson, P Sexton, F Tinsley and M Wilson

Also Present

Councillors O Gunn, A Reed, J Robinson and F Tinsley.

1 Apologies

Apologies for absence were received from Councillors D Bell, G Bleasdale, S Dunn, S Hugill, P Jopling, R Ormerod, J Rowlandson, J Shuttleworth, A Simpson, K Thompson and J Turnbull.

2 Substitute Members

Councillor I Jewell was substituting for Councillor J Turnbull.

3 Minutes

The minutes of the meetings held on 1 February, 1 March and 20 March were agreed as a correct record and signed by the Chair.

4 Declarations of Interest

There were no declarations of interest in relation to any item of business on the agenda.

5 Change to order of business

The Chair of the meeting informed the Committee that he was proposing to change the order of business on the agenda given the number of objectors attending for each item.

Resolved

That the change of the order of business be agreed.

6 Willington - Parking and Waiting Restrictions Order 2019

The Committee considered a report of the Corporate Director of Regeneration and Local Services regarding objections received to a traffic regulation order in Willington (for copy see file of Minutes).

The Strategic Traffic Manager informed the Committee that several requests had been received from local Councillors to address ongoing obstructive parking, visibility and safety issues within Willington, specifically at the following locations:

- Cumberland Terrace
- A690 Commercial Street / High Street
- Chapel Street
- Bridge End

A presentation was shown to the Committee which detailed the locations, together aerial and street view images superimposed with the proposed traffic regulations.

The Strategic Traffic Manager informed the Committee that one objection had been received from a local resident in relation to the proposals for the Cumberland Terrace area regarding the proposed introduction of a 'no waiting at any time' located near to St Thomas RC Primary School. The objector had expressed concern that school staff would park in three allocated parking bays and the resident would not be able to park their vehicle. Furthermore, the resident had been informed by an estate agent who had advised them that their residential property value would reduce by up to £20,000 given the lack of parking. The resident also felt that the proposals would not stop congestion at school times.

The Strategic Traffic Manager explained that the proposals were important in terms of the location of the nearby school. Road safety was an important issue for both the school and parents. The Committee were informed that the width of the street could only accommodate parking for vehicles on one side of the road, however, problems occurred if a vehicle(s) parked on the opposite side of the road.

The local Councillors were then invited to address the Committee. Prior to speaking on the matter, Councillor Tinsley wished for it to be placed on record that a close relative resided at Cumberland Terrace and would be directly affected by the proposals and two of his children attended the local primary school.

Councillor F Tinsley thanked officers for their work and explained that much consultation had taken place. Councillor Tinsley fully supported the proposals for the Chapel Street and Bridge End areas and declared that he was a school governor at Our Lady of St. Thomas School.

In relation to Cumberland Terrace, residents and parents had raised concerns directly to local Councillors and through other forums such as Police and Communities Together (PACT) meetings. Safety issues and problems had been periodically witnessed. The proposals would see an element of parking retained for residents, an extension of the double-yellow lines extending to the A690 would help with visibility splays from Cumberland Terrace. It would also prevent people from parking close to the junction. Councillors had worked very closely with the school and had requested staff not to park in the three allocated bays. Councillor Tinsley explained that ultimately, safety was of paramount importance.

Councillor O Gunn echoed similar comments to Councillor Tinsley and explained that Cumberland Terrace had experienced ongoing problems around the school for a considerable period of time and felt that the proposals would provide a further aspect of safety for children, parents and residents.

The Committee then heard from the objector who lived at Cumberland Terrace. The objector indicated that they lived in house marked 'red' on the presentation. The resident felt that vehicle speeds up and down the street were excessively high and were disappointed that no provision had been made for speed humps. Referring to the three allocated bays, the resident queried where they would park, in the event of the allocated bays being occupied. The resident expressed concern that should their property be placed on the market there would be no parking available. Whilst the resident agreed with the double-yellow line element of the proposals they felt that vehicles would park and wait on the restricted area. The residents had a garage to the rear of the High Street which they could not access due to parked cars. The front door of their property exited directly onto the public footpath and referred to a recent issue whereby a Council vehicle damaged a hanging basket bracket belonging to the resident. The resident felt that the proposals presented would not make a difference and that people would continue to park in the manner already experienced.

In response, the Strategic Traffic Manager explained that the Council were not suggesting that the proposal would be perfect and suitable for all. Highways Officers had worked with all residents throughout the consultation to try and assist. Ultimately, Cumberland Terrace was a very busy area, with highway safety problems and the proposals were an attempt to try and make the situation better.

In response to a query from Councillor P Sexton regarding parking in the back lane, the Strategic Traffic Manager confirmed that vehicles did park along the length of the back lane.

Moved by Councillor K Hopper, **Seconded** by Councillor O Milburn and

Resolved

That the Committee endorse the proposal to proceed with the implementation of the Willington Waiting and Parking Restrictions Order to the Corporate Director of Regeneration and Local Services.

7 Sedgefield – Parking & Waiting Restrictions Order 2019

The Committee considered a report of the Corporate Director of Regeneration and Local Services regarding objections received to a consultation concerning changes to a traffic regulation order in Sedgefield (for copy see file of Minutes).

The Strategic Traffic Manager informed the Committee that several requests had been received from the local Councillors, Sedgefield Town Council, a local school and parents to address ongoing obstructive parking, visibility and safety issues within the Sedgefield area, specifically at the following locations:

- Rectory Row
- Spring Lane
- The Square / High Street
- North End / Whitehouse Drive

A presentation was shown to the Committee which detailed the various locations, together aerial and street view images superimposed with the proposed traffic regulations.

The Strategic Traffic Manager informed the Committee that the physical measures detailed in the report had attracted most comments and objections, as opposed to the traffic regulation order and summarised the proposals in respect of each area.

Rectory Row

The Strategic Traffic Manager informed the Committee that the TRO proposal for Rectory Row was to remove a section of 'restricted waiting 8am-6pm' restrictions (single yellow lines) and remove a section of 'no waiting at any time' restrictions (double yellow lines) on Rectory Row to improve unrestricted parking availability in the area.

The non-TRO element of the proposal would see the introduction of two pedestrian buildouts to improve the safety of pedestrians during school pick up and drop off time. This would improve visibility and distance to cross Rectory Row for school children, parents and other pedestrians.

Two objections were received to the proposals, however, neither objector stated any reasons for their objections throughout the consultation process, both at the informal and formal stages.

Spring Lane

The TRO would include the introduction of double yellow lines to reduce obstructive parking around a junction which were deemed necessary in the interests of road safety to improve the visibility when exiting Spring Lane onto West End. There was alternative unrestricted parking available in the area.

One objection had been received in relation to Spring Lane from a local resident who claims that the obstructions caused by vehicles was not as frequent as the County Council believed.

The Square / High Street

The Strategic Traffic Manager informed the Committee that the proposed changes would include the amendment of the current 'restricted waiting 8am-6pm' restrictions (single yellow lines) and the introduction of 'limited waiting 2hrs no return within 2hrs Mon-Sat 9am-6pm' bays. The aim of the proposal was to try and free up more parking for local businesses and improve parking availability within the town. Existing 'limited waiting 2hrs no return within 2hrs' bays on the High Street would also be harmonised in line with other restrictions in the area. Therefore, the existing restriction would be amended to 'limited waiting 2hrs no return within 2hrs Mon-Sat 9am-6pm'.

One objection had been received to the proposed limited waiting bays on the High Street and felt that the area should remain without restrictions.

The Strategic Highways Manager explained that if the location was occupied by vehicles commuting to work and left for the full duration of the day this would negatively impact on income to the local economy and effect the productivity of the local businesses.

North End / Whitehouse Drive

The Strategic Traffic Manager informed the Committee that the proposal was to improve road safety and reduce obstructive parking on the junction of North Road and White House Drive. The proposed amendments included the alteration of a current 'no waiting at any time' restriction (double yellow lines) and the introduction of a 'no waiting and no loading at any time' (double yellows with kerb ticks).

One objection had been received at both the informal and formal consultation stages which stated that the proposals would displace problematic parking in Sedgefield Town Centre further into residential areas causing problem for residents and in particular White House Drive. The objector also expressed concern that wagons would continue to load and unload in the area of the restrictions as they had no alternative point to do so.

At this point the Committee then heard from objections from those members of the public who had attended the meeting.

The first objector was speaking in relation to the proposals for the Rectory Row area. The resident had two concerns, one of which related to the consultation process. The objector felt that to extend the footway was simply money being wasted on a problem that did not exist. The resident suggested that the money ought to be used to repair potholes in the area instead. In response to the video footage referred to in the report the resident felt that precious resources should not be used because of one instance of bad driving.

Sedgefield was a conservation area with adequate provision for pedestrians and vehicles. Car parking was at a premium in Sedgefield whereby the loss of one car parking space created a problem.

Anti-social behaviour had seen a dramatic rise in the area over recent times and the proposals would encourage people to gather and create further anti-social behaviour.

The Strategic Traffic Manager, referring to the video footage explained that the junction encouraged people to drive in an inappropriate way. The proposal would result in pedestrians having to walk a shorter distance to cross the road safely, where at present any pedestrians had to walk the expanse of the carriageway which was very dangerous and was, in his view, an entirely appropriate proposal from a pedestrian safety point of view.

The Committee then heard from the objector to The Square/High Street proposals. The objector felt that the proposed limited waiting bays on the High Street should be left as they are, without restrictions.

The resident pointed out that Durham County Council planners had approved an application for offices in the area on the basis that staff would utilise on-street parking, however, the proposal detailed in the report would reduce the current on-street parking. The objector also felt that any restriction would cause current users, mainly people who worked locally, to park around access roads to The Square, which would affect residents and businesses causing even more problems experienced at present.

In response to the limited waiting bay, the two-hour limit would operate between 9 a.m. to 6 p.m. under the amended proposals. This would see two bays freed up and enable them to be used to access village facilities. The Strategic Highways Traffic Manager felt that on balance, the proposals would achieve what the Council had set out to do.

The Committee then heard from a local resident who supported the proposal for the Rectory Row area. The resident explained that vehicles undertaking and entering junction on the wrong side of the carriageway occurred on a regular basis. The resident recalled an incident where he had attempted to cross the road at The Golden Lion PH. A vehicle had undertaken a dangerous manoeuvre within the

junction and the objector explained that the vehicle would have collided with him if he had any form of limited mobility. The resident also explained that it was not unusual for vehicle to be parked up for 20 minutes and the occupants eating take-aways. The flats were occupied by elderly residents and Anchor Housing had indicated their support for the proposals which would ensure that dangerous manoeuvres could not be carried out.

The Committee then heard from Councillor J Robinson. Councillor Robinson explained that he lived at Whitehouse Drive and urged the Committee to support the proposals. Spring Lane was located 20 yards from Sedgefield Scout Hut which catered for 500 children on a weekly basis and the improvements were noted.

Councillor Robinson had witnessed the problems of the resident who had come along and expressed support for the proposals and explained that officers and Councillors had tried to devise a solution which would be acceptable to all residents and this had been shown during the consultation.

Councillor Robinson explained that he supported the proposals for the remainder of Sedgefield which would hopefully alleviate some of the problems experienced in an ever growing Town.

Councillor O Milburn explained that she had experienced similar parking issues in her Electoral Division and difficulties with people being able to cross roads. Councillor Milburn expressed her support in favour of the recommendations detailed in the report.

Councillor I Jewell explained that many Councillors had similar issues across the County in towns and villages, often due to the historic road design. Councillor Jewell was not a fan of build-outs, however, he could see why one was being proposed in the area central to peoples concerns in Sedgefield. Councillor Jewell felt that some difficulties could arise but it was very difficult to solve problems for all concerned.

Moved by Councillor O Milburn, **Seconded** by Councillor I Jewell and

Resolved

That the Committee endorse the proposal to proceed with the implementation of the Sedgefield Waiting and Parking Restrictions Order to the Corporate Director of Regeneration and Local Services.

8 Crook - Parking & Waiting Restrictions Order 2019

The Committee considered a report of the Corporate Director of Regeneration and Local Services regarding objections received to a consultation concerning changes to a traffic regulation order in Crook (for copy see file of Minutes).

The Strategic Traffic Manager informed the Committee that the Council were proposing to introduce a number of changes to the parking and waiting restrictions within Crook following requests from businesses and residents, specifically at the following locations:

- Crook Market Place
- Hope Street Car Park
- B6298 / Park Avenue Close
- Rear of Hope Street
- Hope Street
- North Terrace
- Emerson Street/Dawson Street junction
- West End Villas
- Addison Street
- A690/Dale View junction

A presentation was shown to the Committee which detailed the locations concerned, together aerial and street view images superimposed with the proposed traffic regulations.

The Strategic Traffic Manager informed the Committee that the main proposed change for the Crook area related to the proposed introduction of a 3-hour parking limit at Crook Market Place. Parking surveys had indicated that the car park was full by 9am, with around two thirds of the vehicles remaining parked for most of the day. Around 70 out of 90 spaces were occupied by vehicles which did not move for four hours or more. Between 10am and 3pm, the Market Place car park runs at 90-95% capacity. The effect of the time limit would encourage a greater turnover of use and improve the economic vitality of the town. Displaced vehicles would be expected to park in one of the other non-restricted car parks within the town. In addition to the survey work, an additional 81 staff had been relocated to the Civic Centre from other County Council offices which had placed additional pressure on parking availability.

14 objections were received during the initial consultation phase and 10 objections were received during the formal consultation phase. All objections related to the restricted parking proposals (Mon – Sat, 8am - 6pm, 3 hours, no return within 4 hours) at the Market Place car park.

All of the objectors worked in the town and used the Market Place to park daily. The comments received suggested that there were no viable alternative parking areas and felt that their only option would be to park in the surrounding residential streets. It was also noted that a number of objectors felt that the proposals would hinder businesses and their staff and that ultimately the scheme would be counter-productive. It was also argued by objectors that the restrictions would deter customers from using facilities and businesses in the town. One objector was opposed to parking charges being introduced.

The Strategic Traffic Manager explained that the survey work indicated that on average around 65 spaces were available (in each hourly period) in car parks near the Glenholme Boys Club and opposite the Football Club. Both of the car parks could be reached on foot within 5 minutes or so from the town centre and would be a viable alternative option for parking for workers.

The Chair of the meeting explained that one of the local Councillors, Councillor A Patterson was unable to be present at the meeting due to a prior engagement. Councillor Patterson had provided written representation which she had asked to be read out to the Committee in her absence. The representations were read out as follows:

I write in support of the parking restriction proposals for Crook. I am sorry I am unable to attend today's meeting but would like to thank the Chair for allowing me to put forward my comments in my capacity as one of the elected members for Crook and as the Cabinet Portfolio Holder for Corporate Services and Rural Issues.

For many years Crook has had many issues around parking which is no different to other town centres. At present we have limited space available within the immediate town centre for visitors and tourists, which does not support the local economy. Tourists and visitors often struggle to park as there is limited signage and the central parking on the market place is taken up by people who work in the town. The traffic survey data indicates that most spaces are occupied all day limiting spaces for visitors. Meanwhile within a short distance of the town centre we have a high number of spaces left unoccupied for the majority of the day.

I have spoken at length with a number of local businesses and residents and also ran a social media campaign for feedback back in 2018. The majority of residents and businesses were in favour of restricting hours to solve the parking issues but were strongly against introducing parking charges. Those businesses against the restrictions however, did prefer that their customers were able to park on the market place.

Retail is changing in many towns and villages and long term sustainability is a challenge for all. In Crook we have a new Lidl store, an Aldi store under construction, which is due to open later in the year and a new Heron's store. Visitor numbers to Crook from the Dales are increasing for the weekly shop and we are becoming a more attractive "stop off" for visitors travelling to the Dales. We have always felt there was a missed opportunity for Crook to be the "Gateway to the Dales" and parking has always been one of the barriers for attracting visitors.

I note the majority of objections have come from people who work in the town. Whilst I accept that these people will be affected by the proposed changes, the changes would have a positive impact on the Town. People who live and work in the town have the option of walking to work or parking a little further away. It is only

an additional 40-100 m to walk from the other long stay car parks in the surrounding areas. The extra few meters would have a positive impact on health outcomes and free up much needed parking spaces.

When people visit the town they want to be able to park in the most convenient place which is the market place. At present we have people choosing to park on double yellow lines to visit the Barclays cash point as there are no spaces to park.

I also note that there have been several objections from Council staff. We now have more staff based at the Civic Centre in Crook which is helping to support the local economy. As the portfolio holder for Corporate Services I would like to make the committee aware that we have worked with employees who are new to the town to understand where alternative car parks are and we advise all our staff to be good neighbours. It is positive for the town that we now need to limit time in the market place car park and we will work with our staff to ensure that the changes, if agreed, are understood. It is important to us that we are a good employer and a good neighbour.

Introducing the restrictions would have economic benefits, environmental benefits and health benefits and I would urge the committee to support the Officer's recommendations.

The Committee then heard from Councillor A Reed, one of the other local Councillors for the Crook area. Councillor Reed thanked Highways Officers for their hard work in dealing with a very difficult situation. There had been many staffing moves to Crook Civic Centre which inevitably lead to an influx of vehicles. The Market Square was a busy area comprising two bus stops, shops, takeaways, cash machine etc and was a difficult area to navigate during school time and on an evening.

People had complained about being unable to park at the Doctors surgery as a result of other businesses using the same car parking area. The post office at Bridge Street was experiencing similar issues through workers parking vehicles outside and heading off to work.

Councillor Reed explained that Heron Foods, Aldi and Lidl were all being attracted to the area which was extremely good news for Crook. Councillor Reed did express concern for one business located on North Terrace which was adjusting its business model to change its ways of working and offering customers a delivery option. However, the business was having to park several minutes away. Councillor Reed asked if consideration could be given to allow businesses in the area to prosper and assist them with some form of restriction that would allow them to park/load and unload their delivery vehicles.

The Strategic Traffic Manager thanked Councillor Reed for her comments and explained that the matter relating to the business on North Terrace would need to be looked at as a part of a separate issue.

Councillor D Hicks explained that Consett had experienced the same problem with workers using car park spaces in the main car parks and since similar arrangements had been introduced in Consett, the problems had been alleviated and had made a big difference.

Councillor Considine informed the Committee that she was aware of the issues in and around Crook and felt that there was sufficient parking nearby.

Councillor I Jewell felt that the proposals presented a positive move forward and the Council was doing its best for the area.

Moved by Councillor J Considine, **Seconded** by Councillor D Hicks and

Resolved

That the Committee endorse the proposal to proceed with the implementation of the Crook Waiting and Parking Restrictions Order to the Corporate Director of Regeneration and Local Services

9 Newton Aycliffe – Parking & Waiting Restrictions Order 2019

The Committee considered a report of the Corporate Director of Regeneration and Local Services objections received to the consultation regarding changes to a traffic regulation order in Newton Aycliffe (for copy see file of Minutes).

The Strategic Traffic Manager informed the Committee that the Council had received several requests to address ongoing obstructive parking, visibility and safety issues at the following locations within the Newton Aycliffe area:

- Hurworth Road
- Millennium Way
- Long Tens Way
- Greenfield Way
- Welbury Grove
- Middridge Village

A presentation was shown to the Committee which detailed the locations concerned, together aerial and street view images superimposed with the proposed traffic regulations.

The Committee were informed that the proposals for all areas would see the introduction of a 'no waiting at any time' restriction (double yellow lines) and summarised the proposals in respect of each area.

Hurworth Road

Proposals had been requested by local business owners to enable current and potential new businesses to have access within and around the Hurworth road. The area was currently unrestricted, which meant that vehicles often parked and blocked access for vehicles and HGV's. Some vehicles are believed to be owned by staff commuting to work and are parked for the full duration of the day.

Two objections had been received and were detailed in the report. The Strategic Traffic Manager explained that the proposals would allow for loading and unloading for local businesses and for people to drop off and pick up passengers. The restrictions would improve accessibility, visibility and reduce the amount of obstructive all-day parking.

The objectors argue that the restrictions would adversely affect their businesses, however it is the council's belief that the wider benefits for the businesses on the estate outweigh such adverse impacts for these two objectors.

Middridge Village Hall

Measures were requested by Middridge Parish Council to address obstructive parking near the Village Hall, which would improve movement of vehicles around the area.

One person had objected to the proposals on the basis that cars parked were not prohibiting access and were only doing so due to a lack of off-street parking. The objector felt that examples of obstructive parking were experienced elsewhere in the village.

Millenium Way / Moordale Road / Long Tens Way

The proposals were requested by local business owners in a bid to reduce obstructive parking and improve road safety. There had been no objections to the proposal.

Welbury Grove

The proposals had been requested by local residents in a bid to reduce obstructive parking and improve road safety. There had been no objections to the proposal.

Greenfield Way

The proposals were to improve road safety with regards to parked vehicles on Greenfield Way as per the request from both local County Councillors. There had been one objection to the proposal. The objector was pleased that the Council were attempting to resolve an ongoing problem but felt that the only way to solve the 'chaotic' traffic problems would be to have a traffic management scheme that compelled all arrivals and departures from the school to follow a single route.

In response the Strategic Traffic Manager explained that the proposal of a traffic scheme to create a one-way system using undeveloped land would be costly and could potentially cause delays with commuters and road users of the current heavily trafficked area. The current proposals would improve the safety of pedestrians and road users with an instant benefit.

Councillor K Hopper explained that Greenfield Way was one of the main arterial routes in and out of the area and carried a lot of traffic and could understand why the area needed to be improved from a safety point of view.

Moved by Councillor I Jewell, **Seconded** by Councillor O Milburn

Resolved

That the Committee endorse the proposal to proceed with the implementation of the Crook Waiting and Parking Restrictions Order to the Corporate Director of Regeneration and Local Services.

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DURHAM COUNTY COUNCIL

At a Meeting of **Highways Committee** held in the Richardson Suite, The Glebe Centre, Murton on **Monday 2 September 2019 at 11.00 a.m.**

Present:

Councillor C Kay in the Chair.

Members of the Committee:

Councillors D Bell, G Bleasdale, S Dunn, K Hopper, S Hugill, J Maitland, O Milburn, S Morrison, A Napier, J Rowlandson, P Sexton, J Shuttleworth and J Turnbull.

Also Present:

Councillors J Maitland and A Napier.

1 Apologies for Absence

Apologies for absence were received from Councillors J Considine, D Hicks, K Liddell, R Ormerod, A Simpson, K Thompson and M Wilson.

2 Substitute Members

There were no substitute members present.

3 Declarations of Interest, if any

There were no declarations of interest in relation to any items of business on the agenda.

4 Proposed diversion of Public Bridleway No. 29, Murton Parish – Town and Country Planning Act 1990 Section 257

The Committee considered a joint report of the Corporate Director of Regeneration and Local Services and Head of Legal and Democratic Services regarding the proposed diversion of public bridleway no. 29 (Murton Parish) under Section 257 of the Town and Country Planning Act 1990 (for copy see file of Minutes).

The Access and Rights of Way Team Leader informed the Committee that the bridleway proposed for diversion became a public right of way in 2004 when it was legally created as part of the reclamation of the site and north-easterly direction across the site commencing in nearby South Hetton and roughly followed the line of the former mineral railway.

In July 2019 full planning permission was granted for phase one of the development of the Jade Business Park, a major economic project in East Durham to bring employment to the site of the former Hawthorn Colliery which was reclaimed after the closure of the pit in 1991.

An application to divert the bridleway was received in May 2019 under the Town and Country Planning Act which provides for a local authority to make a public path diversion order providing it is satisfied that to do so would enable development to be carried out in accordance with planning permission. The existing route of bridleway 29 would conflict with the development of phase one of the Jade Business Park and the proposed diversion was the minimum necessary to enable the development to proceed. The Committee were informed that the proposed diversion would follow a southerly route around the outside of the southern edge of the development which had been observed on the site visit prior to the meeting. The Committee noted through their site visit that the proposed diversion would maintain the integrity of the route and keep path users away from the development area and minimise potential interaction with vehicles. The Access and Rights of Way Team Leader referred to the initial groundworks that had commenced on site, noted during the site visit and explained that the work had been carried out following a temporary closure order and should the application be rejected at any stage, the bridleway would have to be reinstated.

One objection to the proposal had been received from the Ramblers Association. Their objection was on the basis that the proposed diversion was significantly longer and may be an inconvenience to walkers. No objections were received from any of the other consultees which included the local councillors, local user groups, Murton Parish Council and the British Horse Society.

In terms of the objection received the Committee were informed that it was felt that the statutory test had been satisfied. The development could not proceed without diverting the public bridleway. The proposed diversion was approximately 263 meters longer than the current route and was the minimum diversion feasible to avoid the development site. It was considered that the additional distance did not make the alternative route unsuitable, given that it was a recreational route more than 3km long between South Hetton and 2.5km between South Hetton and Murton therefore an additional 263 metres was not deemed significant to people walking for recreational purposes.

Alternative options had been considered. It was not considered feasible to retain the route through the development site and there was no safe way of accommodating the public bridleway due to the size and access requirements of phase one of the business park development. A diversion to the north was considered but this would potentially require moving again in the future as the development of the site progressed. A much larger diversion to the south of the site

was also considered, however, this was felt to be beyond the scope of a diversion order under the provisions of section 257 of the Town and County Planning Act.

The Committee then heard from Councillor A Napier, one of the local Councillors for the area.

Councillor Napier provided an insight into the history of the site where upwards of 2,500 people were once employed, during the mining era. The Committee heard how unemployment in East Durham rose to nearly 35% following the pit closure programme. Thirty years on, the economy in the area had never recovered to the level it had once enjoyed, prior to the pit closure programme. Councillor Napier felt that facts made it all the more imperative to access the site and have it developed as quickly as possible.

Councillor Napier explained that had it not been for the global economic crash in 2008, the site would have been developed. The opportunity present today was another chance to meet the aspirations of the area. Planning approval had been agreed, funding was secured, and the first phase of the site was ready to be developed which would bring much needed employment to the area.

Councillor Napier asked the Committee to support the recommendations. Councillor Napier also appealed to the representative from the Ramblers Association to reconsider and withdraw their objection and give due consideration to the history of the site and what it would mean for Hawthorn, Murton and East Durham.

Councillor Napier explained that two supervised walking groups both of which operated from the Glebe Centre at Murton had offered no objections to the diversion and could not understand why the Ramblers Association had chosen to object to the proposal. No other user groups had raised objections. Councillor Napier explained that the employment and economic benefits associated with the development were plain to see. There would be in the region of 2000 jobs created on the site, culminating in much needed employment and future employment for the children and grandchildren of those who had worked within the mining industry.

Councillor J Maitland supported the comments made by Councillor Napier and explained that the route of the diversion was one of the original paths in the area. Jobs were vital for not only Murton but the surrounding area.

The Committee then heard from the representative of the Ramblers Association who explained that it gave him no pleasure to be the only objector to the proposal. The main objection was that the track to be diverted had been in place for a considerable amount of time both as a public bridleway and one-time, mineral railway. The path was also detailed on a 1970's historic map of the area. The Ramblers Association felt it was a great shame that the path could not be preserved for those reasons. The diversion was a significantly longer route. The Association did not wish to be in the position of standing in the way of bringing jobs

to the area and were very much aware of the importance of jobs in the area and understood how mining areas had suffered with employment. The Association felt that some way of preserving the path and its history, through story boards about the colliery and the area would have benefitted the area considerably. Under the circumstances, for the reasons outlined, the Ramblers Association wished to maintain their objection.

The Access and Rights of Way Team Leader advised the Committee that they could not reconsider the planning decision.

The Committee then heard from a representative speaking on behalf of the applicant. The representative thanked the Access and Rights of Way Team Leader for his succinct presentation and the endorsements from the two local Councillors. The applicant explained that the key issue related to the nature of the site. The site had always been an employment site, as a coking and coal preparation site which had sat vacant for the last 20 years. The applicant had been contracted by the local authority to assist with the marketing and development of the land to bring in much needed employment, which they had been very successful in doing. The applicant explained that part of the development had been pre-let. However, that pre-let would disappear if the current objection was sustained. There was a key timeline related to the project and substantial costs would be incurred if there was any deferment of the development. This combined with the current potential and other letting interest would ultimately mean that an extraordinary opportunity would be lost for the area. The applicant referred to the cultural heritage, mentioned by the Ramblers Association and explained that they were very alert and receptive to those issues which could hopefully be dealt with by way of separate dialogue as to how the heritage of the site could be promoted to enable benefit to all users. In summing up, the applicant explained that the bridleway could never be sustained through the middle of the site as it would create a danger for users. The diversion onto the National Cycle Network was screened from the development and was very much considered a pleasant route. In terms of the diversion, a mere, additional 263 metres of a 3km walk was not deemed excessive. The applicant clearly endorsed the views pronounced for the need to divert the bridleway, highlighting that it was being moved to a better location with no rights being extinguished. The applicant also confirmed that the diversion was supported by the equestrian movement.

Councillor J Shuttleworth supported the recommendation and explained that the statutory test had been met and there were sound economic and employment reasons to divert the footpath.

Councillor S Dunn explained that the proposal under consideration was for phase one of the development and suggested that other routes may require diversion in the future. Councillor Dunn fully supported the proposal before the Committee, however, he felt a more holistic approach could have been taken in providing for the future expansion of the Jade site. Councillor Dunn echoed Councillor Napier's request to the Ramblers Association to reconsider their objection.

Moved by Councillor J Shuttleworth and **Seconded** by Councillor J Turnbull.

Resolved

That a Diversion and Definitive Map Modification Order be made in respect of Public Bridleway No. 29 Murton Parish, under the provisions of section 257 of the Town and Country Planning Act 1990 and section 53(A)(2) of the Wildlife and Countryside Act 1981, and subsequently either confirmed or referred to the Secretary of State for determination.

5 Proposed extinguishment of part of Public Footpath No. 27, Murton Parish - Highways Act 1980 Section 118

The Committee considered a joint report of the Corporate Director of Regeneration and Local Services and Head of Legal and Democratic Services regarding a proposal to extinguish part of Public Footpath No. 27 Murton Parish by public path order (Highways Act 1980 Section 118) at Hesledon Moor East (for copy see file of Minutes).

The Access and Rights of Way Team Leader informed the Committee than an application had been received from the owner of Hesledon Moor East to extinguish a section of Public Footpath No. 27 on the grounds that it was not needed for public use. A consultation on the proposed extinguishment had resulted in an objection from the Ramblers Association. The public footpath No. 27 Murton Parish ran in a southerly direction from Footpath No. 18, through a yard at Hesledon Moor East, to join Footpath No.26, before heading east and eventually finishing near the A19. The path was linked to a network of public rights of way to the south and east of Murton. Members walked the length of the path proposed for extinguishment during a site visit prior to the meeting.

The Access and Rights of Way Team Leader explained that the proposal would result in a reduction of the network by approximately 220 metres. In terms of the test for confirmation of an unopposed Order, it was considered that the section of path was unlikely to be used by the public, and the extinguishment would not have a negative effect on the landowner, who was the applicant. Public Footpaths Nos.18 and 26 provided an alternative route for pedestrians, avoiding farmyard areas.

The Committee were informed that in terms of an alternative, the only alternative to an extinguishment of the footpath would be a Diversion Order, under the provisions of section 119 of the Highways Act, in the interests of the landowner and on the grounds of privacy and security. However, this option had been considered by the applicant and rejected.

In conclusion the Access and Rights of Way Team Leader explained that it was considered that the statutory tests for making of an extinguishment order and thereafter confirmation as an unopposed order, were met.

Councillor Napier, speaking as one of the local Councillors was speaking in support of the application. Councillor Napier explained that there was clear evidence that the path was not used for public use. The two businesses located at the farm had endured a significant amount of anti-social behaviour, theft and arson attacks. Given that a perfectly adequate alternative route had been identified, Councillor Napier appealed to the Ramblers Association to withdraw their objection. Councillor J Maitland echoed the concerns expressed by Councillor Napier and had walked the alternative path on many occasions, which was far a more pleasant and enjoyable recreational route which was not overly lengthy.

The Committee then heard from the representative of the Ramblers Association who appreciated that the applicant may find it inconvenient for a public footpath to run through the middle of his farm, however, there were many similar situations where this type of situation arose. The Ramblers Association also felt that anti-social behaviour was not of good enough reason to extinguish a footpath, as there were many other paths across the network that encountered similar issues. The route was also significantly longer as opposed to walking straight through the farm. The Ramblers Association appreciated that the applicant was going to improve the short section of Footpath No. 26, however, the general view remained that there was a clear route through the farm buildings, and it was not a difficult path to take.

The Committee then listened to representations from the applicant who explained that they had sought the extinguishment for a number of reasons. The applicant explained that the footpath would have been closed on health and safety grounds had it been any other business or industry, particularly with the movement associated with livestock, tractors and wagons etc. The businesses on site had seen an increased fire risk in the area and highlighted that there had been four fires over recent times. The applicant told the Committee how they had to park trailers at either end of the footpath to secure property during the evening, not to block the footpath, but to block access to off road vehicles which had also caused issues. The Police had advised the applicant to pursue closure of the footpath given the issues being encountered.

The Access and Rights of Way Team Leader explained to the Committee that there was an obvious motivation for the applicant to pursue the footpath extinguishment, however, the Committee were reminded that it was not a qualifying criterion that could be used to close the path, the relevant legal test being whether the path was needed for public use. The Committee were also informed that the applicant had offered to carry out improvements to public footpath no. 26, should the application be successful. However, this was a voluntary offer and no weight could be afforded to it in the consideration of the application. It was very rare for the County Council to support and indeed, proceed with extinguishment orders. However, in this case it

was felt that the objector had not addressed the legal test of whether the footpath was unnecessary for public use and had not offered any evidence to suggest that the path was needed for public use to justify refusal of the application. In this case there was a perfectly adequate alternative route in footpaths 18 and 26, and it was considered that the section of path in question was not needed for public use.

Councillor J Shuttleworth felt that the statutory tests for the making of the extinguishment order had been met, given the information presented by way of the report and following the comments made at the meeting.

Councillor P Sexton explained that in this particular case, he found it very difficult to understand why a footpath user would want to walk through a middle of a working farm which contained plant machinery, livestock and dogs, particularly when there was a more amenable and perfectly adequate alternative route that footpath users could enjoy.

Councillor S Dunn commented that he fully understood why the Ramblers Association wanted to preserve public footpaths and bridleways and supported their aspirations to increase and improve them. Personally, Councillor Dunn felt that it was inappropriate for footpaths to lead through farms not only for the safety of footpath users but also for workers on site. It was also important to note the potential increased crime risk in this case. Councillor Dunn felt that the proposed extinguishment in this case was valid as the current part of the footpath wasn't particularly needed. There was also no evidence of any meaningful use. As a side issue Councillor Dunn felt that the County Council should implement a Rights of Way Improvement Plan detailing aspirations for improvement which could lead to beneficial and strategic improvements to the public path network.

The Solicitor for Planning and Development acknowledged the point made by Councillor Dunn in relation to the Rights of Way Improvement Plan but explained that it was not within the remit of the Committee for the matters under consideration and suggested that this be dealt with as a separate matter by way of dialogue with relevant officers at a later date.

Moved by Councillor J Shuttleworth, **Seconded** by Councillor P Sexton and

Resolved

That an Extinguishment and Definitive Map Modification Order be made in respect of Public Footpath No. 27 Murton Parish, under the provisions of section 118 of the Highways Act 1980 and section 53(A)(2) of the Wildlife and Countryside Act 1981, and subsequently either confirmed or referred to the Secretary of State for determination.

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Highways Committee

25 September 2019

**Coxhoe – Parking & Waiting
Restrictions ORDER 2019**



Report of Corporate Management Team

Ian Thompson Corporate Director of Regeneration and Economic Development.

Councillor Carl Marshall, Cabinet Portfolio Holder for Regeneration and Economic Development.

Electoral division(s) affected:

Coxhoe

1 Purpose of the Report

- 1.1 In accordance with part 3 of the Council's Constitution, Members are asked to decide in principle only which will then guide the Corporate Director in the exercise of delegated decision making. The final decision is therefore one for the Corporate Director, under delegated powers.
- 1.2 To advise Members of objections received to the consultation concerning changes to the traffic regulation order in Coxhoe.
- 1.3 To request that members consider the objections made during the informal and formal consultation period.

2 Executive Summary

- 2.1 Within Civil parking Enforcement operational guidance, the County Council are committed to regularly reviewing traffic regulation orders to ensure that the restrictions held within them are relevant and appropriate.
- 2.2 The County Council are proposing to introduce restrictions on:

2.3 Church Street

The proposed restrictions ('Limited Waiting' parking bays, 'Loading Only' parking bays, removal of 'Blue badge holders only' bays and amendment to the duration

of 'Restricted Waiting' restrictions) are to enable customers for current and potential new businesses to have an area to park and have access to the local businesses and amenities. The restrictions are to stop all day and commuter parking as this reduces revenue within the town. The majority of areas are currently unrestricted, some vehicles are believed to be owned by staff commuting to work or who leave their vehicles in this location for the full duration of the day and commute outside of Coxhoe.

The proposals were requested by local business owners and the local County Councillors in a bid to reduce obstructive parking, improve road safety and to boost the local economy.

2.4 Coronation Terrace

The proposed amendments to 'restricted waiting' restrictions which are currently in place Mon-Fri 7am-6pm are to be amended to Mon-Sat 7am-6pm to be the same as the other proposed restrictions within the town. This will reduce obstructive parking and improve traffic flow.

The proposals were requested by the local County Councillor in a bid to harmonise the restrictions within the town, reduce obstructive parking and improve road safety.

2.5 Roundabout A177 south of Cornforth Lane

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by the Local County Councillors, local residents and the school headteacher to address obstructive parking near to Coxhoe Primary School, which should improve movement of vehicles on the roundabout and improve road safety for school children and pedestrians.

2.6 Welfare Terrace

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by local residents and the local County Councillors in a bid to reduce obstructive parking and improve road safety on the corner and junction of Welfare Terrace and Cornforth Lane.

3 Recommendation(s)

3.1 Committee is recommended to:

Endorse the proposal in principle to proceed with the implementation of the Coxhoe: Parking and Waiting Restrictions Order. With the final decision to be made by the Corporate Director under delegated powers.

4 Background

- 4.1 Several requests have been received to address ongoing obstructive parking, visibility and safety issues within the Coxhoe area.
- 4.2 The initial consultation exercise was undertaken with statutory consultees and directly affected frontages from February 2019 to April 2019.
- 4.3 There were objections received at this point from directly affected frontages. Support was received from Durham Constabulary and Local County Councillors. A decision was made to progress with the TRO (Traffic Regulation Order) and move to the formal consultation stage.
- 4.4 The proposals were advertised formally on site and in the local press between 30th May 2019 – 20th June 2019. There was a formal objection received during this consultation period from a directly affected frontage.

5 Church Street

The proposed restrictions ('Limited Waiting' parking bays, 'Loading Only' parking bays, removal of 'Blue badge holders only' bays and amendment to the duration of 'Restricted Waiting' restrictions) are to enable customers for current and potential new businesses to have an area to park and have access to the local businesses and amenities. The restrictions are to stop all day and commuter parking as this reduces revenue within the town. The majority of areas are currently unrestricted, some vehicles are believed to be owned by staff commuting to work or who leave their vehicles in this location for the full duration of the day and commute outside of Coxhoe.

The proposals were requested by local business owners and the local County Councillors in a bid to reduce obstructive parking, improve road safety and to boost the local economy.

There is support from Durham Constabulary. There have been 8 objections to the proposals.

Objection 1 (Objected at the informal consultation stage)

Objector 1 is a local business owner and resident of Church Street and has objected at the informal consultation stage.

They state that they run a hair dressing business and a lot of their customers are in the shop for more than 3 hrs.

Objection 2 (Objected at the informal and formal consultation stage)

Objector 2 is a resident of Church Street and has objected at the informal and formal consultation stage.

They state as a resident with no access to park at the rear of their property that they would have nowhere to park.

Objection 3 (Objected at the informal consultation stage)

Objector 3 is a local business owner of Church Street and has objected at the informal consultation stage.

They state that they can't see how restrictions are going to help residents and businesses, we need more parking spaces. Also, how will the restrictions be policed?

Objection 4 (Objected at the informal consultation stage)

Objector 4 is a local business owner of Church Street and has objected at the informal consultation stage.

They state that there are too many parking restrictions everywhere in the County.

Objection 5 (Objected at the informal consultation stage)

Objector 5 is a local resident of Church Street and has objected at the informal consultation stage.

They request to have permit parking for their family, so they can park outside their home, as everyone will try and park in the layby as it has no parking restrictions on the plan.

Objection 6 (Objected at the informal consultation stage)

Objector 6 is a local business owner of Church Street and has objected at the informal consultation stage.

They state that their neighbour abuses the blue badge scheme and is not disabled, and always use their bay that they require for unloading.

Objection 7 (Objected at the informal consultation stage)

Objector 7 is a local business owner of Church Street and has objected at the informal consultation stage.

They state that if the proposals are carried out that they will close their business immediately. The system as it stands works, why the necessity to change what

has worked fine for many years. They state that in all the years of business they have never witnessed any problems with parking, traffic congestion or road accidents.

They allege that there are a numerous petty restrictions on parking outlined (all negative) but not one suggestion (positive) as to where those displaced from all day parking, i.e. business owners should leave their vehicles. The objector states that there is also no access to the rear of these properties, which leaves the car park at the village hall. Objector 7 continues to state that the majority of businesses open their doors for 9am and will presumably park at the village hall along with the following:

1. People who work in Durham and use it as a 'park and ride' the only difference being a work colleague picks them up instead of using public transport.
2. Local car dealers who often park cars for several weeks, although this has eased since legislation regarding car tax and the selling of vehicles.
3. The recently introduced 2hr waiting restriction on the village hall car park is ignored by the 'Durham park and ride brigade' and everyone else for that matter. The treat of overstaying still applies however.

They state that they look forward to receiving plans of the new car park you will be building to alleviate the chaos your proposals will, no doubt cause.

Objection 8 (Objected at the informal consultation stage)

Objector 8 is a local business owner and resident of Church Street and has objected at the informal consultation stage.

They state that in principle they agree whole heartedly with the parking restrictions along Church Street, being a shop owner and living here that it brings frustration especially when potential shoppers cannot park.

They continue to state as they live here without some sort of permit parking that the restrictions will cause countless issues. It will mean moving their vehicle 6 days a week, every morning and bringing it back each evening to park. They state they need to load their van during the summer months as they attend events, even if they managed to obtain a place in the proposed loading space, can you imagine the number of trips made to and from their van.

The objector cannot see why as the only resident I am not given some sort of parking permit.

Response

The proposals were sent out to statutory consultees as required by secondary legislation, informal plans and letters sent to all directly affected frontages and formally advertised online and on site as per the legal requirements.

The proposed restrictions are to enable customers for current and potential new businesses to have an area to park and have access to the local businesses and amenities. The restrictions are to stop all day and commuter parking as this reduces revenue within the town. The majority of areas are currently unrestricted, some vehicles are believed to be owned by staff commuting to work or who leave their vehicles in this location for the full duration of the day and commute outside of Coxhoe.

The proposed 'limited waiting' restrictions will allow for short stay parking for customers of the local businesses, blue badge holders can park in these locations for an unlimited duration. Loading and unloading can also take place in these locations. The restrictions will reduce the amount of all-day parking.

At the south end of Church Street which is the main shopping location, the 'limited waiting' proposal are to have a 30min limit so that customers can park in this section for a short duration and will provide a high turnover of vehicles and income to the town. This will also allow for vehicles wishing to load/unload a higher chance of access. The rest of the 'limited waiting' restrictions will have a 3hr limit as requested by the local business owners, this will allow customer enough time to use the shops and businesses.

The proposed 'loading only' bays will give access to vehicles wishing to load and unload throughout the day. Vehicles will not be able to park in these bays which give full access for deliveries.

The proposed amendments to 'restricted waiting' restrictions which are currently in place Mon-Fri 7am-6pm are to be amended to Mon-Sat 7am-6pm to be in keeping with other proposed restrictions within the town. This will reduce obstructive parking and improve traffic flow.

The proposed removal of 'disabled badge holders only' bays and the introduction of 'limited waiting' restrictions will not affect blue badge holders as they are permitted to park in any 'limited waiting' bays for an unlimited duration. The proposed 'limited waiting' bays will then give access to any vehicle wishing to shop within the town.

Only restrictions on Church Street (High Street) has the 'limited waiting' restrictions of up to 3hrs. Any vehicles wishing to park for a duration over 3hrs can park in any of the nearby streets which are unrestricted. It's in the best interest of the local businesses to have available short stay parking near to their

shop/business to allow customers easy access. If the area is full of long stay/all day parked vehicles, potential customers may not stop and therefore shop elsewhere.

Permit parking would not achieve the aim of providing customer parking, as the situation would not change, and the location would be full of residents and business owner vehicles which would reduce potential customers parking availability.

If business owners, residents or members of the public find that vehicles are abusing the restrictions, then they need to report this to Durham County Council who will be able to arrange for targeted enforcement to take place.

5.1 Coronation Terrace

The proposed amendments to 'restricted waiting' restrictions which are currently in place Mon-Fri 7am-6pm are to be amended to Mon-Sat 7am-6pm to be the same as the other proposed restrictions within the town. This will reduce obstructive parking and improve traffic flow.

The proposals were requested by the local County Councillor in a bid to harmonise the restrictions within the town, reduce obstructive parking and improve road safety.

There is also support from Durham Constabulary. There has been 1 objection to the proposals.

Objection 1 (Objected at the informal consultation stage)

The objector is a local resident of Coronation Terrace. They state that the parking bays get used throughout the day for businesses up the road and are usually full, objector 1 states that they only have outside their homes to park.

Response

The proposals were sent out to statutory consultees as required by secondary legislation, informal plans and letters sent to all directly affected frontages and formally advertised online and on site as per the legal requirements.

The proposed amendments to 'restricted waiting' restrictions which are currently in place Mon-Fri 7am-6pm are to be amended to Mon-Sat 7am-6pm to be the same as the other proposed restrictions within the town. This will reduce obstructive parking and improve traffic flow.

There are parking bays along the east side of Coronation Terrace for the full length and will remain unrestricted so that residents, customers and business owners can park here for an unlimited duration.

5.2 Roundabout A177 south of Cornforth Lane

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by the Local County Councillors, local residents and the school headteacher to address obstructive parking near to Coxhoe Primary School, which should improve movement of vehicles on the roundabout and improve road safety for school children and pedestrians.

There is support from Durham Constabulary. There have been no objections to the proposals.

5.3 Welfare Terrace

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by local residents and the local County Councillors in a bid to reduce obstructive parking and improve road safety on the corner and junction of Welfare Terrace and Cornforth Lane.

There is support from Durham Constabulary. There have been no objections to the proposals.

6 Conclusion

6.1 It is recommended that Members agree in principle to endorse the proposal to proceed with the implementation of the Coxhoe: Waiting and Parking Restrictions Order. With the final decision to be made by the Corporate Director under delegated powers.

7 Background papers

7.1 Correspondence and documentation in Traffic Office File:

\\dch-3900\arc2-sys2\$\TRAFPROJ\06 REGULATION DESIGN & IMPLEMENTATION\Settlement\Coxhoe\Traffic Regulation Orders (Parking Restrictions)\2019 January

Contact: Peter Broxton

Tel: 03000 263986

Appendix 1: Implications

Legal Implications

All orders have been advertised by the County Council as highway authority and will be made in accordance with legislative requirements.

Finance

LTP Budget.

Consultation

Is in accordance with SI:2489.

Equality and Diversity / Public Sector Equality Duty

It is considered that there are no Equality and Diversity issues to be addressed.

Human Rights

No impact on human rights.

Crime and Disorder

This TRO will allow effective management of traffic to reduce congestion and improve road safety.

Staffing

Carried out by Strategic Traffic.

Accommodation

No impact.

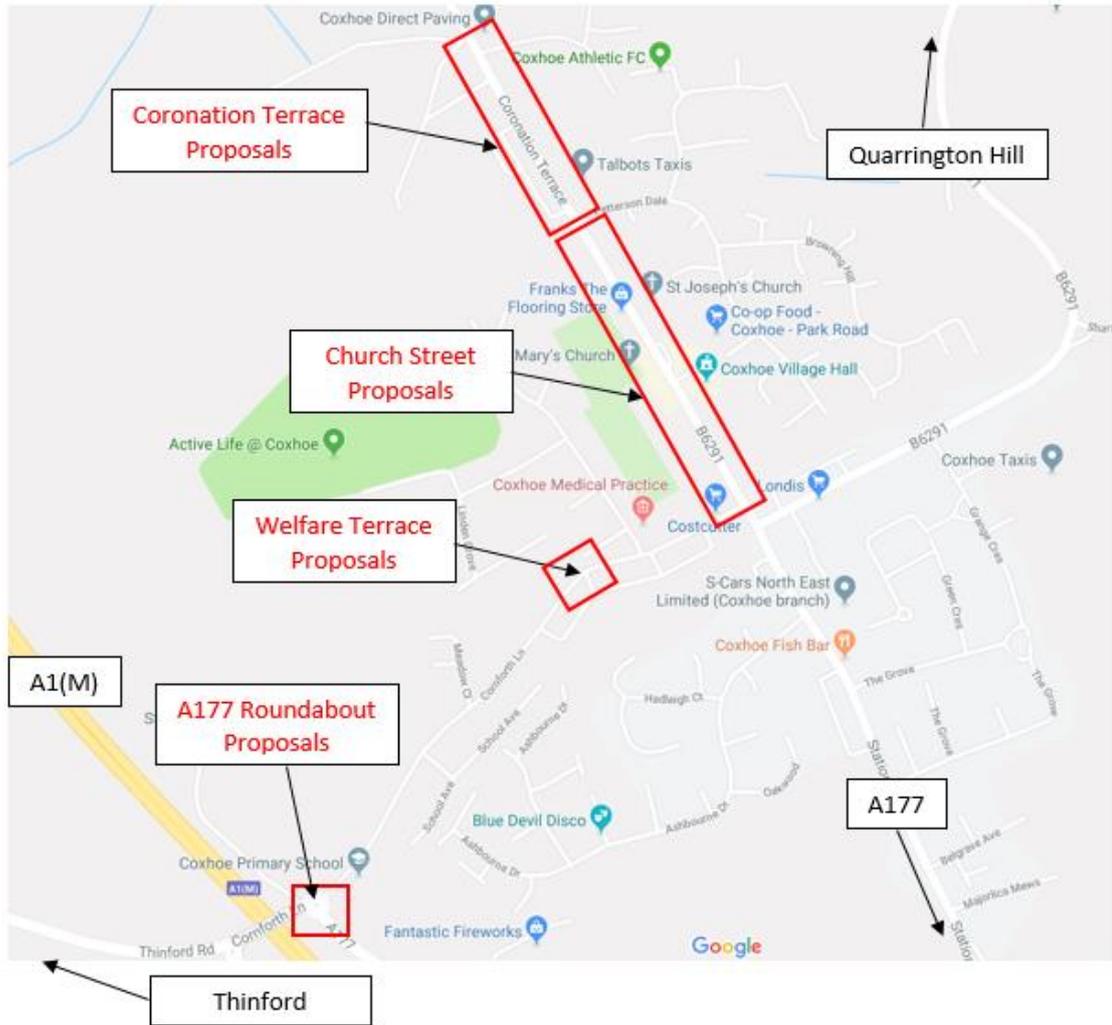
Risk

Not Applicable.

Procurement

Operations, DCC.

Appendix 2: Location of Proposals



Coxhoe Parking & Waiting Restrictions Order 2019

Highways Committee
25th September 2019

Altogether better



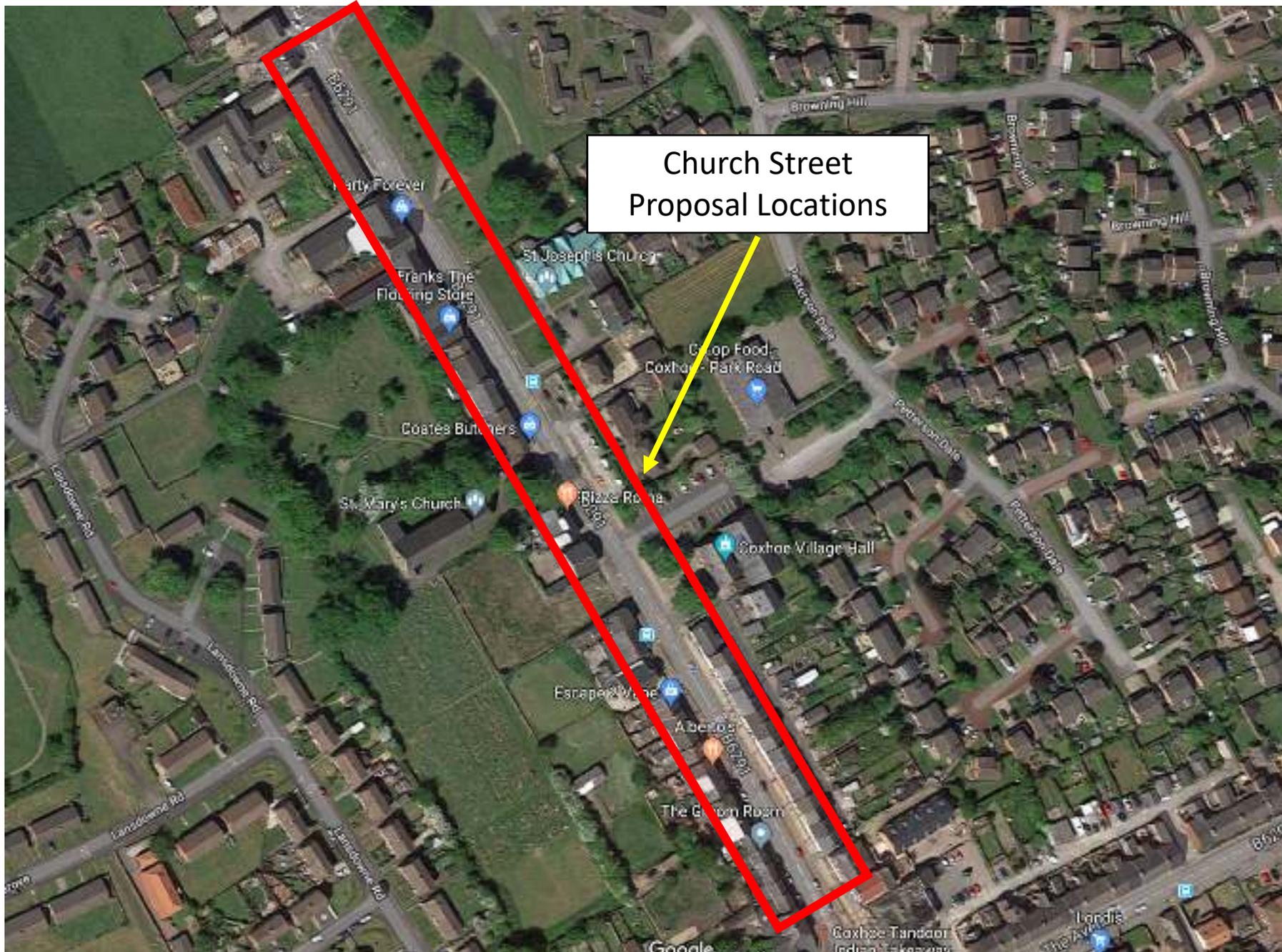
Location Plan – Coxhoe Area



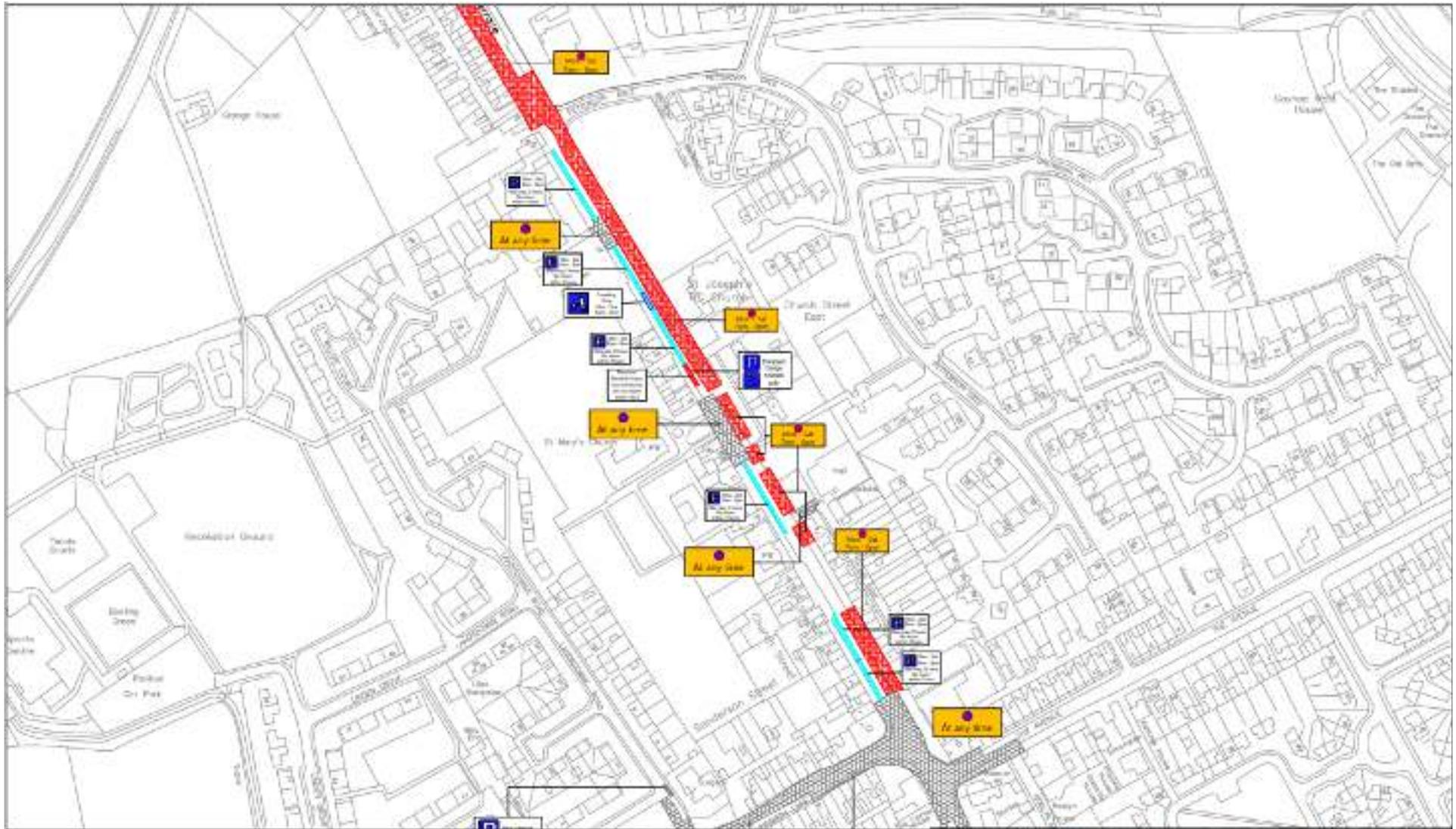
Altogether better



Church Street Proposal Locations



Church Street Proposals (Overview)



Key to Types of Restriction

- Proposed Restricted Waiting: Changing duration from Mon-Fri to Mon-Sat
- Proposed Restricted Bay
- Existing No Waiting At Any Time

Map Grids
Grid Reference Number

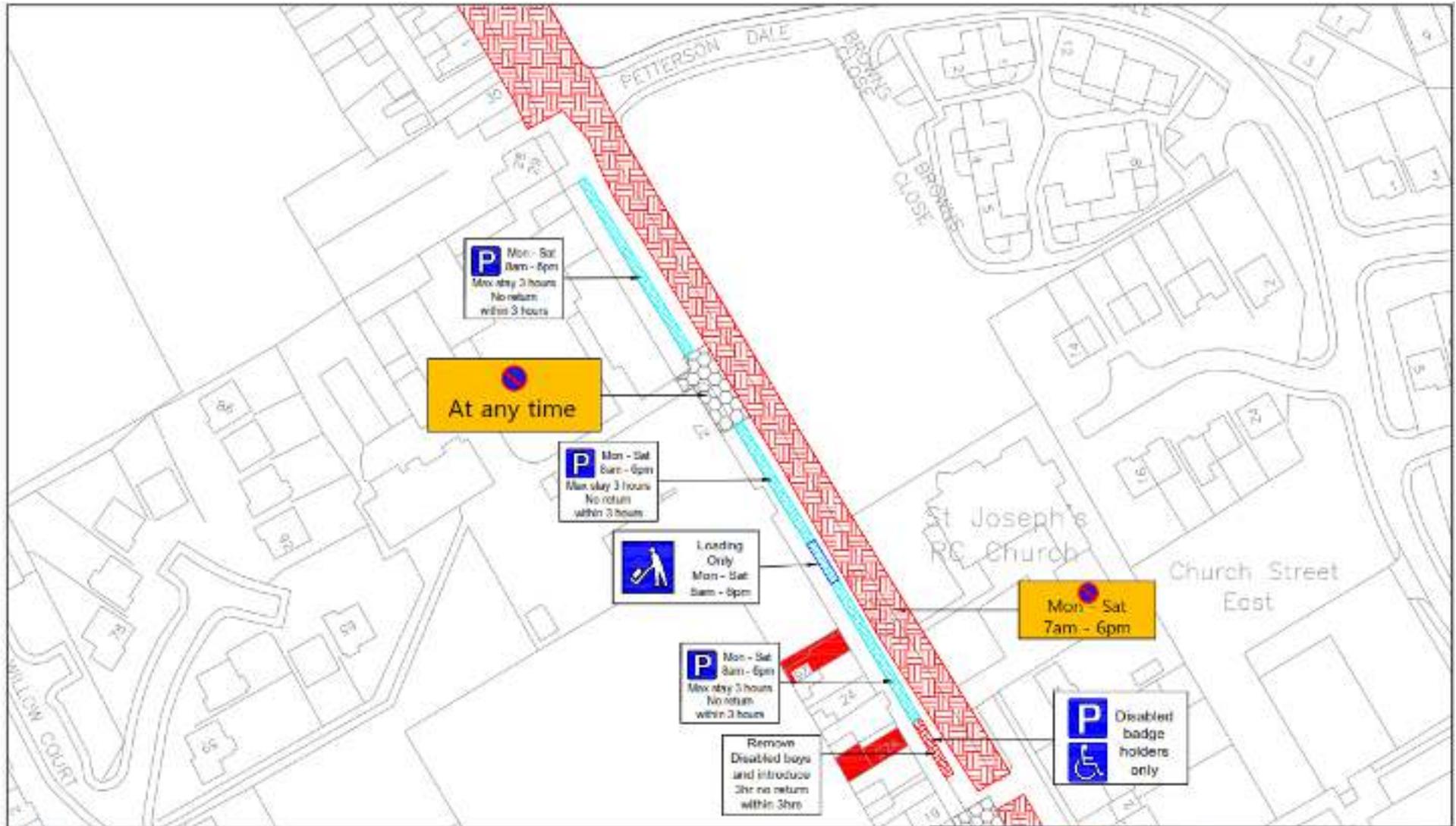
Durham County Council
Corporate Director
Regeneration & Economic
Development
Strategic Traffic Management
County Hall, Durham DH1 1SU

**THE COUNTY COUNCIL OF DURHAM - COXHOE
PARKING AND WAITING RESTRICTIONS
PROPOSED**

	Title:	1-1000 @ A3	
	Date:	February 2018	
	Drawn by:	R. Brown	Signature
	Date Checked:	Map Submission	#114

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Proposals and Objectors – Church Street (North)



Key to Types of Restriction

- Proposed Restricted Walking
- Proposed Restricted Lay
- Existing No Waiting At Any Time
- Objection

Map Based Schedule Number



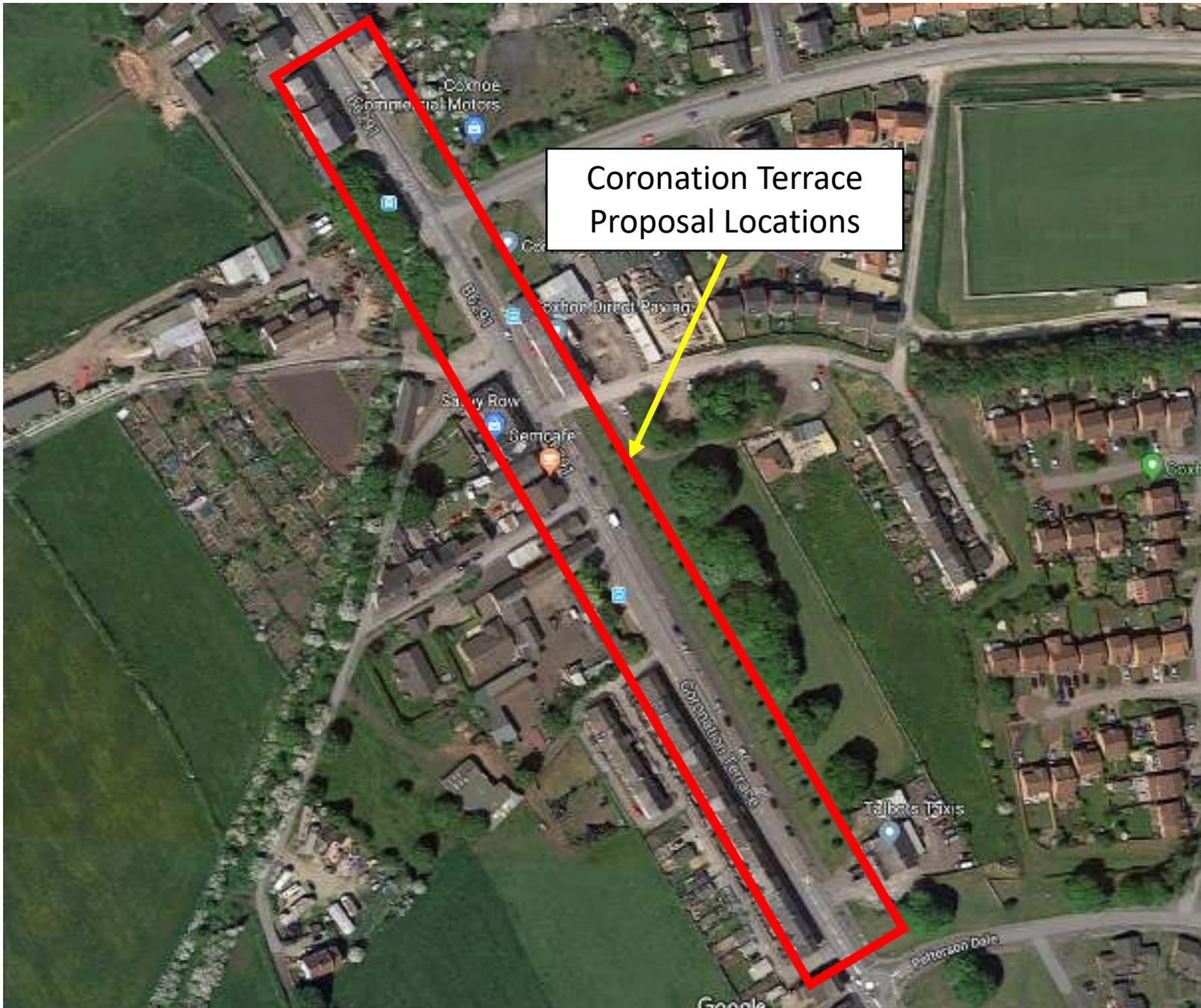
Corporate Director
 Regeneration & Economic
 Development
 District Traffic Management
 County Hall, Durham DH1 1JG

THE COUNTY COUNCIL OF DURHAM - COXNDE PARKING AND WAITING RESTRICTIONS PROPOSED

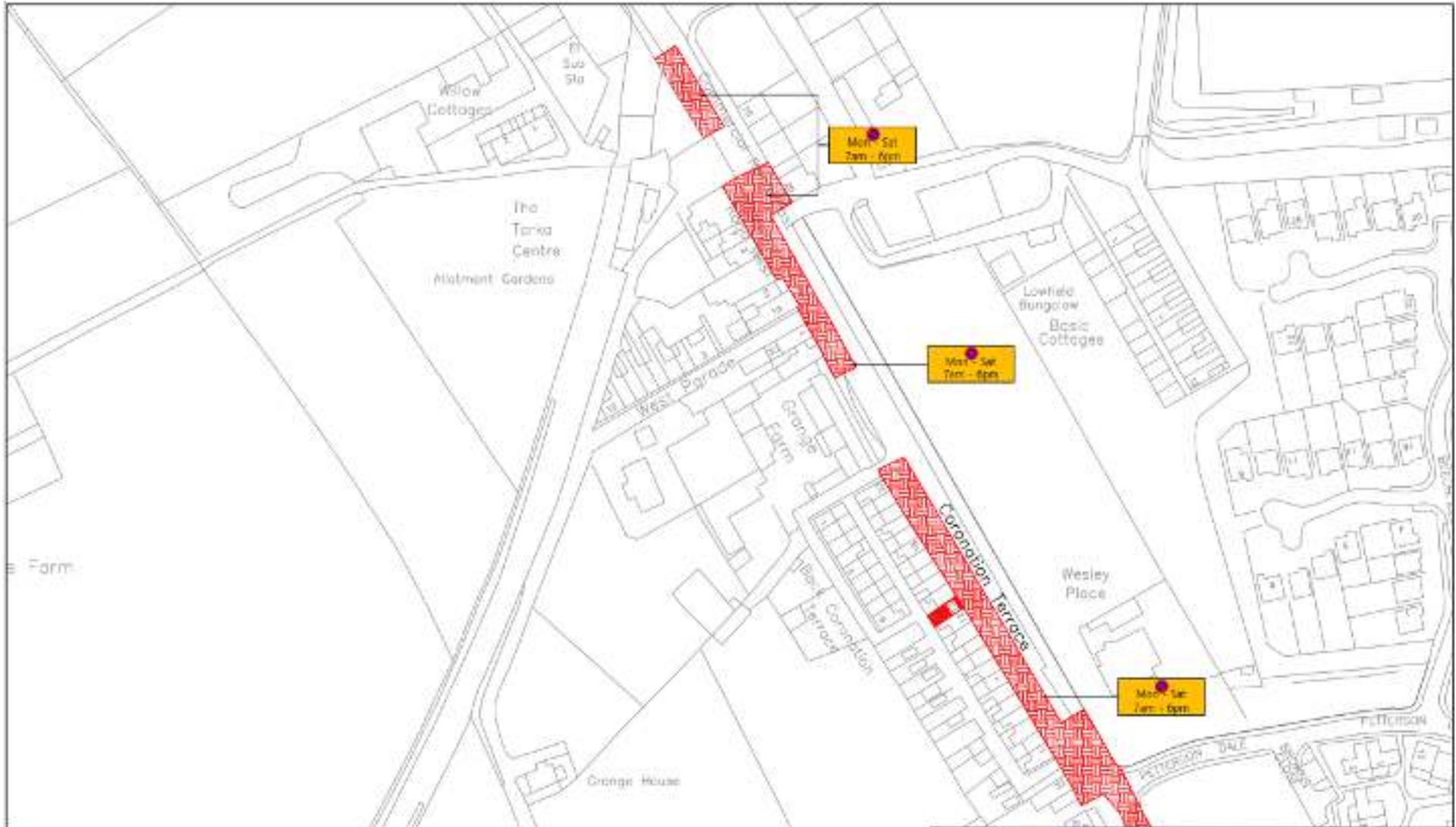


Scale:	1:100 @ A3
Date:	February 2015
Drawn by:	L. GIBSON
Checked by:	Map Designer
Date issued:	01/14

Coronation Terrace - Proposal Locations



Proposals and Objectors – Coronation Terrace



Key to Types of Restriction

 Proposed Restricted Waiting
Changing duration from Mon-Fri
to Mon-Sat

 Objectors

See also
schedule number



Corporate Director
Regeneration & Economic
Development

Strategic Traffic Management

County Hall, Durham DH1 1TA

COXHOE PARKING AND WAITING RESTRICTIONS PROPOSED



Scale:	1:1250 @ A1
Date:	February 2010
Drawn by:	F. Brown
Checked by:	MBG
Drawn by:	MBG
Checked by:	BH101

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Roundabout A177 South of Cornforth Lane Proposals



Proposals and Objectors – Roundabout A177 South of Cornforth Lane



Key to Types of Restriction

-  Proposed No Waiting At Any Time
-  Waiting No Stopping

Map Based
Ordnance Number



Durham County Council

Corporate Director
Regeneration & Economic
Development

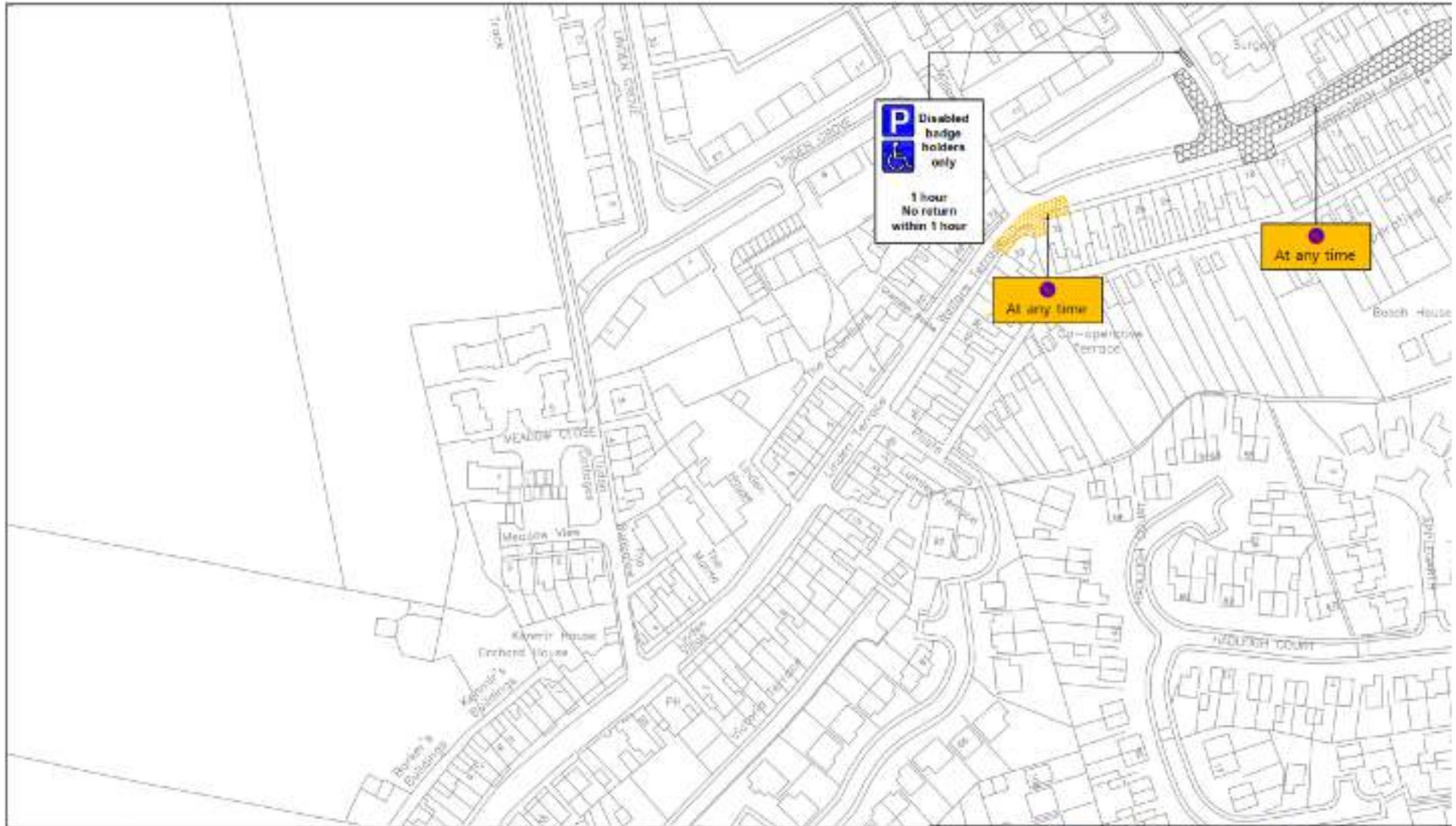
Strategy: Traffic Management
County Hall, Durham DH1 1TA

COXHOE PARKING AND WAITING RESTRICTIONS PROPOSED		
Scale	1:1200 @ A3	
Date	February 2019	
Drawn by	R. Niven	Signature:
Date Issued	Map Structure	BM12

Welfare Terrace - Proposals



Proposals – Welfare Terrace



Key to Types of Restriction

-  Proposed No Waiting At Any Time
-  Existing No Waiting At Any Time
-  Existing Restricted Area

Site Based
Schedule Number



Corporate Director
Regeneration & Economic
Development
Strategic Traffic Management
County Hall, Durham DH1 1JD

THE COUNTY COUNCIL OF DURHAM - COXHOE PARKING AND WAITING RESTRICTIONS PROPOSED



Scale:	1:1250 @ A3
Date:	February 2015
Drawn by:	E. Pinner
Date Checked:	MAC R. Smith

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Highways Committee

25 September 2019

Barnard Castle – Parking & Waiting
Restrictions ORDER 2019



Report of Corporate Management Team

Ian Thompson Corporate Director of Regeneration and Economic Development.

Councillor Carl Marshall, Cabinet Portfolio Holder for Regeneration and Economic Development.

Electoral division(s) affected:

Barnard Castle East and West

1 Purpose of the Report

- 1.1 In accordance with part 3 of the Council's Constitution, Members are asked to decide in principle only which will then guide the Corporate Director in the exercise of delegated decision making. The final decision is therefore one for the Corporate Director, under delegated powers.
- 1.2 To advise Members of objections received to the consultation concerning changes to the traffic regulation order in Barnard Castle.
- 1.3 To request that members consider the objections made during the informal and formal consultation period.

2 Executive Summary

- 2.1 Within Civil parking Enforcement operational guidance, the County Council are committed to regularly reviewing traffic regulation orders to ensure that the restrictions held within them are relevant and appropriate.
- 2.2 The County Council are proposing to introduce restrictions on:

2.3 Bridge End/ The Sills

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by local residents as parked vehicles are affecting movements at the Traffic Signals, the proposals are in a bid to reduce obstructive parking and improve road safety.

2.4 Birch Road

The TRO is to introduce a Limited Waiting parking bay 9am-6pm, 30 minutes no return within 2hrs, which was requested by local residents to improve parking availability and reduce long stay commuter parking. Also, to amend a section of 'School Keep Clear' markings to reflect what is actually on the ground for St Mary's Primary School.

2.5 Market Place

The TRO is to amend an existing Limited Waiting bay 9am-6pm 2hrs no return before 6pm to a disabled badge holder only bay, which was requested by the Town Council to improve disable access to the town.

2.6 Bridgagate/ The Bank

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines). The proposals were requested by the Traffic Safety Team of Durham County Council in a bid to reduce obstructive parking and improve road safety.

3 Recommendation(s)

3.1 Committee is recommended to:

Endorse the proposal in principle to proceed with the implementation of the Barnard Castle: Parking and Waiting Restrictions Order. With the final decision to be made by the Corporate Director under delegated powers.

4 Background

- 4.1 Several requests have been received to address ongoing obstructive parking, visibility and safety issues as well as improve parking in Barnard Castle.
- 4.2 The initial consultation exercise was undertaken with statutory consultees and directly affected frontages between December 2018 and January 2019.
- 4.3 A further informal consultation exercise was undertaken with directly affected frontages in June 2019 to ensure all frontages had a chance to view the proposals.
- 4.4 Several objections were received at this point from directly affected frontages. The proposals were supported by Durham Constabulary. After reviewing the feedback, a decision was made to progress with the Traffic Regulation Order (TRO) and move to the formal consultation stage.
- 4.5 The formal notices for the proposals were advertised on site and in the local press between 31st July 2019 – 21st August 2019. One further objection was received at this point.

5 Bridge End/ The Sills

The Sills is a 2-way road on the B6277 leading north to Barnard Castle Bridge and the A67, it is a 3-way junction controlled by Traffic Signals. There are residential properties on The Sills and Bridge End as well as leading into Ullathorne Rise residential estate.

The TRO is to introduce 'no waiting at any time' restrictions (double yellow lines) on the east side of the carriageway from the existing restrictions opposite to no.17 in a southerly direction for 36m. The proposals were requested by local residents as parked vehicles are affecting movements at the Traffic Signals causing queuing issues. The proposals are in a bid to reduce obstructive parking, congestion, improve road safety and traffic flow.

There were reports of vehicles parking south of the existing double yellow lines on the east side of The Sills. When vehicles are travelling north along The Sills waiting at a red light and vehicles are travelling south onto The Sills from Barnard Castle Bridge or A67 southbound are then unable to pass the parked vehicles due to queuing traffic, thus causing queues onto the Bridge which has a negative effect on the flow of traffic through the traffic signals.

Durham Constabulary support the proposals. There have been 5 objections and 6 representations in favour of the proposals.

6 Objections

All objections come from residents who live in the Riverside properties on the east side of The Sills where they park their vehicles as they have no off-street parking.

6.1 Objection 1

Objector 1 is a Riverside resident and states that they have no parking outside their property and the proposal will force them to park even further away.

6.2 Objection 2

Objector 2 is a Riverside resident and objected to the proposals with no further comment.

6.3 Objection 3

Objector 3 is a Riverside resident and states that parking on Riverside is already at a premium and would only result in parking opposite onto the residential housing estate which is overcrowded. They also state that the road provides over spill parking for people visiting the riverside and Barnard Castle.

6.4 Objection 4

Objector 4 is a Riverside resident and state that they are concerned at the possibility of any changes to the parking arrangement, though mindful of traffic requirements. They request that any changes be kept to a minimum.

6.5 Objection 5

Objector 5 is a Riverside resident and states that the decision to purchase the permanent residency was due to the suitability of the property, access to parking of their car and to the local town.

They continue to state that if they go shopping and have several loads to transport to their property, they unlock their door and ferry back and forth to the current parking spot in view from their property. However, if they must park further away, they will not feel happy leaving their front door unlocked while carrying out repeat loads.

They state that the joy they have from living there will be severely affected, also poor weather will affect the longer walks to the property. They state that they suffer from bad backs on regular occasions and not able to speed up to avoid being soaked by the wet weather, unlock the door repeatedly.

They acknowledge that the issue is due to drivers not looking ahead and seeing what vehicles are parked and whilst irritating is not a danger.

They state that parking issues by delivery drivers and visitors fill up Ullathorne Rise which in their view is a danger.

They suggest that permits are issued to permanent residents of 1-5 Riverside as there is space for three cars, which could be a temporary measure to see if that will resolve the issue prior to introducing double yellow lines.

6.6 Response

The current parking of vehicles in the proposed location is deemed as obstructive parking, causing issues to the productivity of the traffic signals and traffic flow around the three-way signalised junction.

The requirement for the extension to the double yellow lines outweighs the requirement for residents to park next to their properties as the proposals are in the interest of road safety and to improve traffic flow.

The double yellow lines will still allow for vehicles to board and light (drop off and pick up passengers), allow vehicles to load and unload (for deliveries and to unload shopping etc) and allows disabled badge holders to park on them for a 3hr duration as long as the vehicle is not causing an obstruction to the highway.

There is unrestricted parking in Ullathorne Rise and south of the proposals on the B6277 The Sills.

Residential permit parking would not be suitable in this location as it would have the same effect as the current situation, it would also not meet the relevant criteria as per Durham County Councils Parking Policy where permit areas need to be in close proximity of 3rd party generators (commercial businesses).

7 Birch Road

Birch Road is a residential area within walking distance to A67 market place, the high street and close proximity to St Mary's Primary School.

There have been requests to introduce a short stay parking bay to help with school pick up and drop off and for vehicles to park to use the local amenities within Barnard Castle.

The TRO is to introduce a 'Limited Waiting' parking bay Mon-Sat 9am-6pm, 30 minutes no return within 2hrs on the north side of Birch Road outside no.1 for one bay length of 6m. It is also proposed to remove a section of 'School

Keep Clear' markings to represent the true reflection on the ground, on the north side of Birch Road from no.1 westerly for 13m.

Durham Constabulary support the proposals. There has been 1 objection and 2 representations in favour of the proposals.

8 Objections

8.1 Objection 1

The objector is a resident of Birch Road. They state that the area needs residential permit parking not half-hearted ideas.

8.2 Response

The introduction of the limited waiting bay will give a short stay parking space for local residents to use the facilities within the town and reduce long stay parking near to the school keep clear markings. The parking bay will also allow parents of the school children to have access to drop off their child, which will reduce congestion at school drop off and pick up times.

The introduction of residential permit parking would not meet Durham County Councils Parking Policy as the location is not in the direct area of 3rd party generators and the policy also states that permit parking will not be introduced to address problems associated with school gate parking.

9 Market Place

The A67 Market Place is the high street for Barnard Castle and is a heavily trafficked area with a high density of pedestrians using the local amenities.

There have been requests from Barnard Castle Town Council to amend an existing 'Limited Waiting' bay near to the Kings Court Care Home to a 'blue badge holder only' bay to allow better access for disabled badge holders.

The TRO is to introduce one blue badge holders bay on the west side of Market Place south of the entrance to Kings Court car home for one bay length of 6m.

There has been support from Durham Constabulary for the proposals. There have been no objections to the proposals.

10 Bridgeway/ The Bank

The Bank/ Bridgeway is part of the A67 and is a heavily trafficked area on a bend south of the Market Cross. There has been works done on the section

between the bend and the market cross to improve parking availability by creating bus stop pedestrian buildouts.

A request has been received from a Durham County Council Traffic Safety Team to ensure the safety and visibility of road users around this stretch of carriageway by introducing parking restrictions.

It is therefore proposed to introduce 'no waiting at any time' restrictions (double yellow lines) from no.1 Bridgegate continuing east around to no. 53 on The Bank for 43m.

There has been support from Durham Constabulary for the proposals. There have been no objections to the proposals.

11 Conclusion

11.1 It is recommended that Members agree in principle to endorse the proposal to proceed with the implementation of the Barnard Castle: Waiting and Parking Restrictions Order. With the final decision to be made by the Corporate Director under delegated powers.

12 Background papers

12.1 Correspondence and documentation in Traffic Office File:

\\dch-3900\arc2-sys2\$\TRAFPROJ\06 REGULATION DESIGN & IMPLEMENTATION\Settlement\Barnard Castle\Traffic Regulation Orders (Parking Restrictions)\2018 Amendments

Contact: Peter Broxton

Tel: 03000 263986

Appendix 1: Implications

Legal Implications

All orders have been advertised by the County Council as highway authority and will be made in accordance with legislative requirements.

Finance

LTP Budget.

Consultation

Is in accordance with SI:2489.

Equality and Diversity / Public Sector Equality Duty

It is considered that there are no Equality and Diversity issues to be addressed.

Human Rights

No impact on human rights.

Crime and Disorder

This TRO will allow effective management of traffic to reduce congestion and improve road safety.

Staffing

Carried out by Strategic Traffic.

Accommodation

No impact.

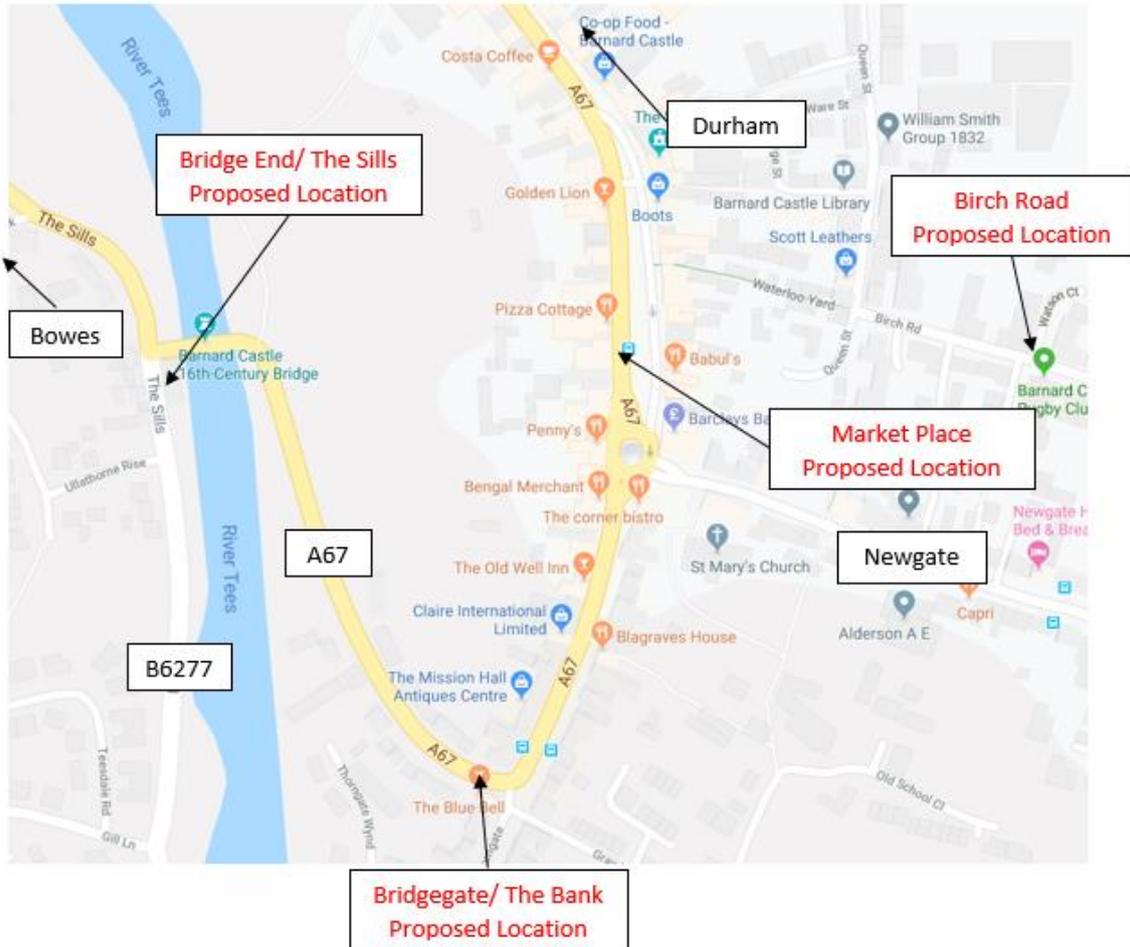
Risk

Not Applicable.

Procurement

Operations, DCC.

Appendix 2: Location of Proposals



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Barnard Castle Parking and Waiting Order 2019

Highways Committee
25th September 2019

Altogether better



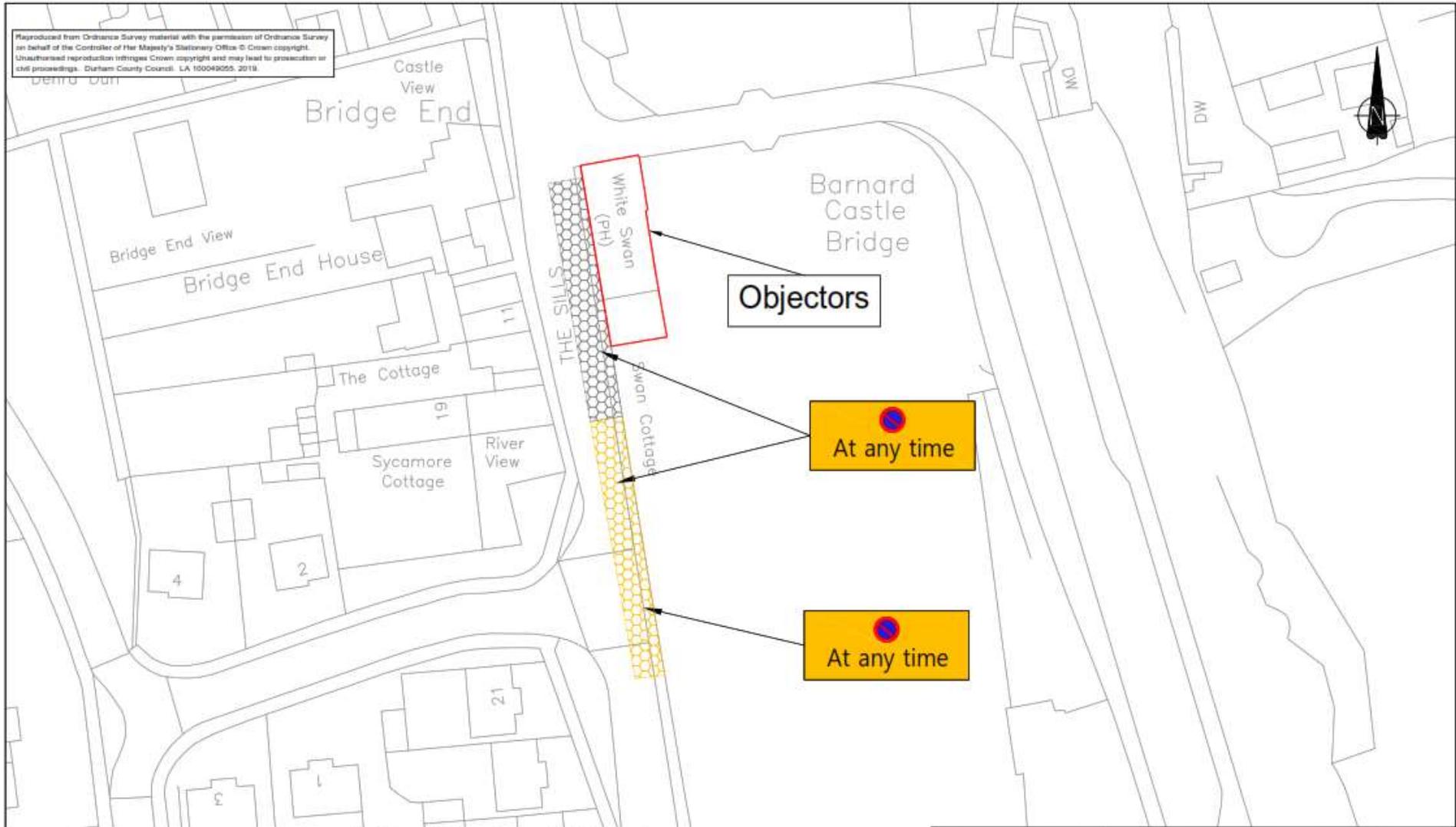
Location Plan – Barnard Castle Area

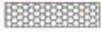


Altogether better



Proposal and Objectors – Bridge End/ The Sills

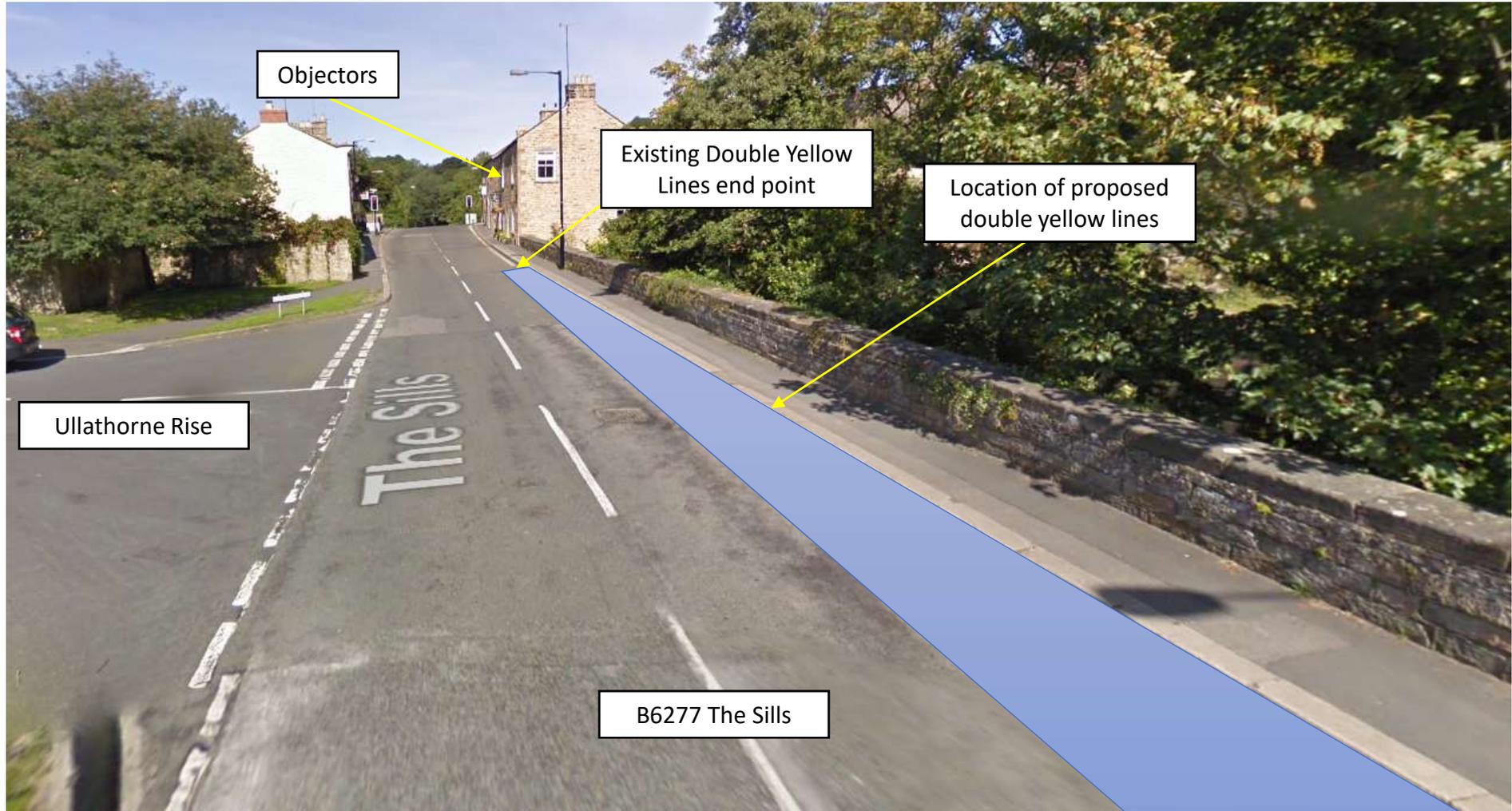


 Proposed No Waiting At Any Time  Existing No Waiting At Any Time	Rev/ Amdt	Drawn By		Approved By			Description Of Amendments
		Name	Date	Name	Date	Sign	
		P. Broxton	June 19				

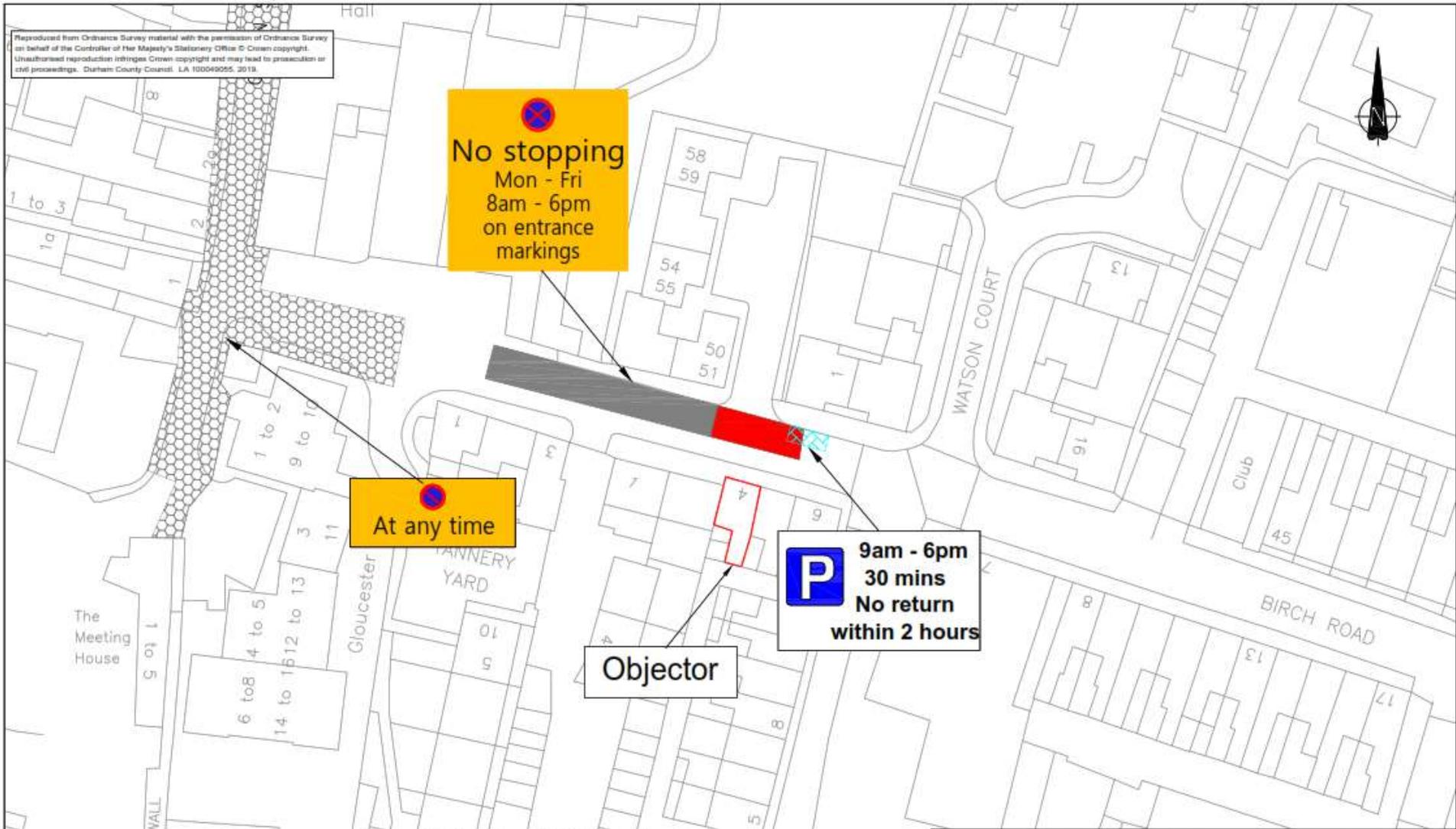
 Durham County Council STRATEGIC TRAFFIC MANAGEMENT CORPORATE DIRECTOR OF REGENERATION & LOCAL SERVICES COUNTY HALL, DURHAM, DH1 5YQ		Project Barnard Castle Drawing PROPOSED
Scales 1/1250 @ A3	Project/Drawing No. CE43	
		Rev.

PATH:

Proposal and Objectors – Bridge End/ The Sills



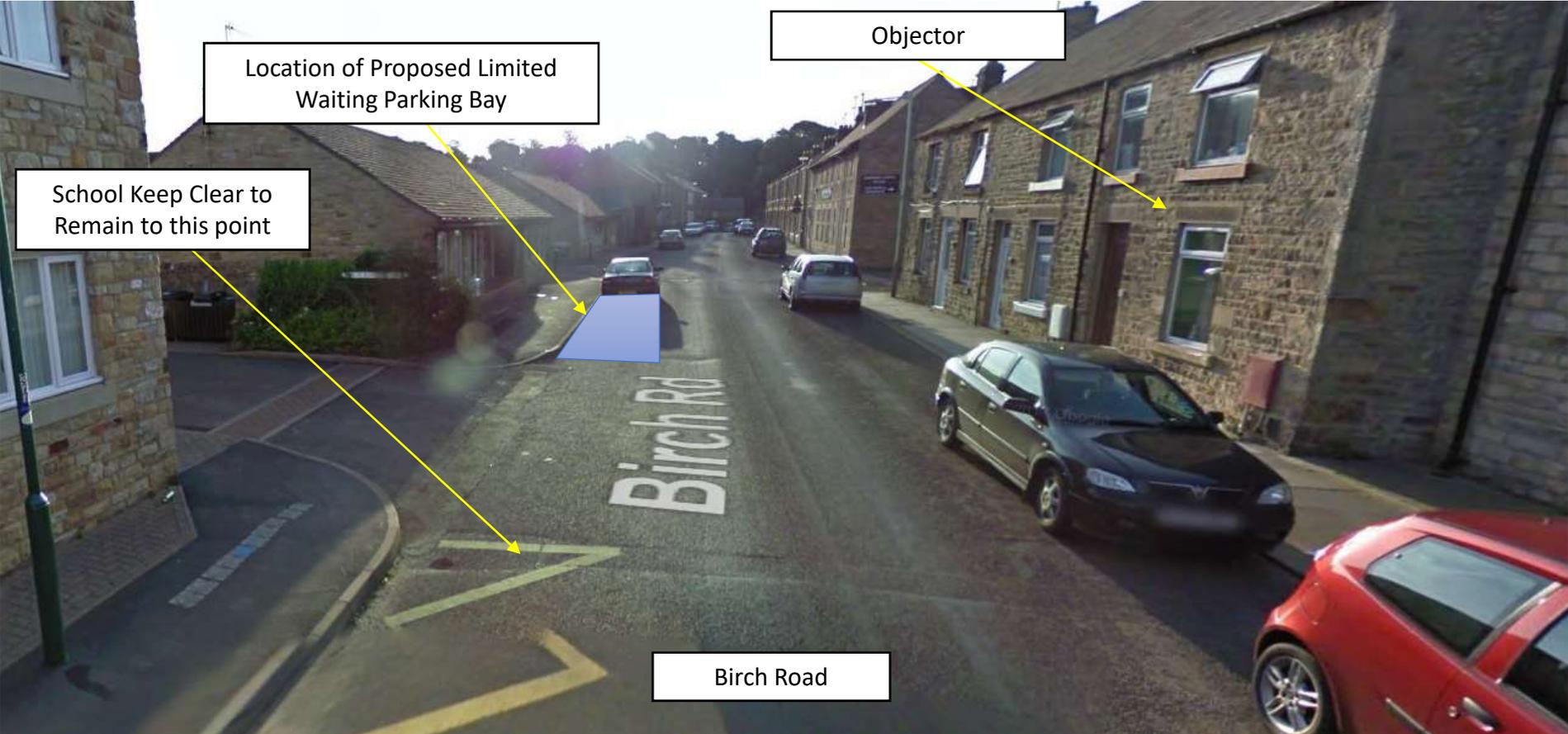
Proposal and Objector – Birch Road



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	Rev/ Amdt	Drawn By		Approved By			Description Of Amendments
		Name	Date	Name	Date	Sign	
		P. Broxton	June 19				
STRATEGIC TRAFFIC MANAGEMENT CORPORATE DIRECTOR OF REGENERATION & LOCAL SERVICES COUNTY HALL, DURHAM, DH1 1TA				Project Barnard Castle		Drawing PROPOSED	
				Scales 1/1250 @ A3		Project/Drawing No. CF43	
PATH:							
Rev.							

Proposals and Objector – Birch Road



Proposal– Market Place No Objections



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Legend:

- Proposed Restricted Bay
- Existing No Waiting and No Loading/Unloading At Any Time
- Existing No Waiting At Any Time
- Existing Restricted Bay

PATH:

Rev/ Amdt	Drawn By		Approved By			Description Of Amendments
	Name	Date	Name	Date	Sign	
	P. Braxton	June 19				

Durham
County Council

STRATEGIC
TRAFFIC
MANAGEMENT

CORPORATE DIRECTOR OF
REGENERATION & LOCAL
SERVICES

COUNTY HALL, DURHAM, DH1 1TA

Project: Barnard Castle

Drawing: PROPOSED

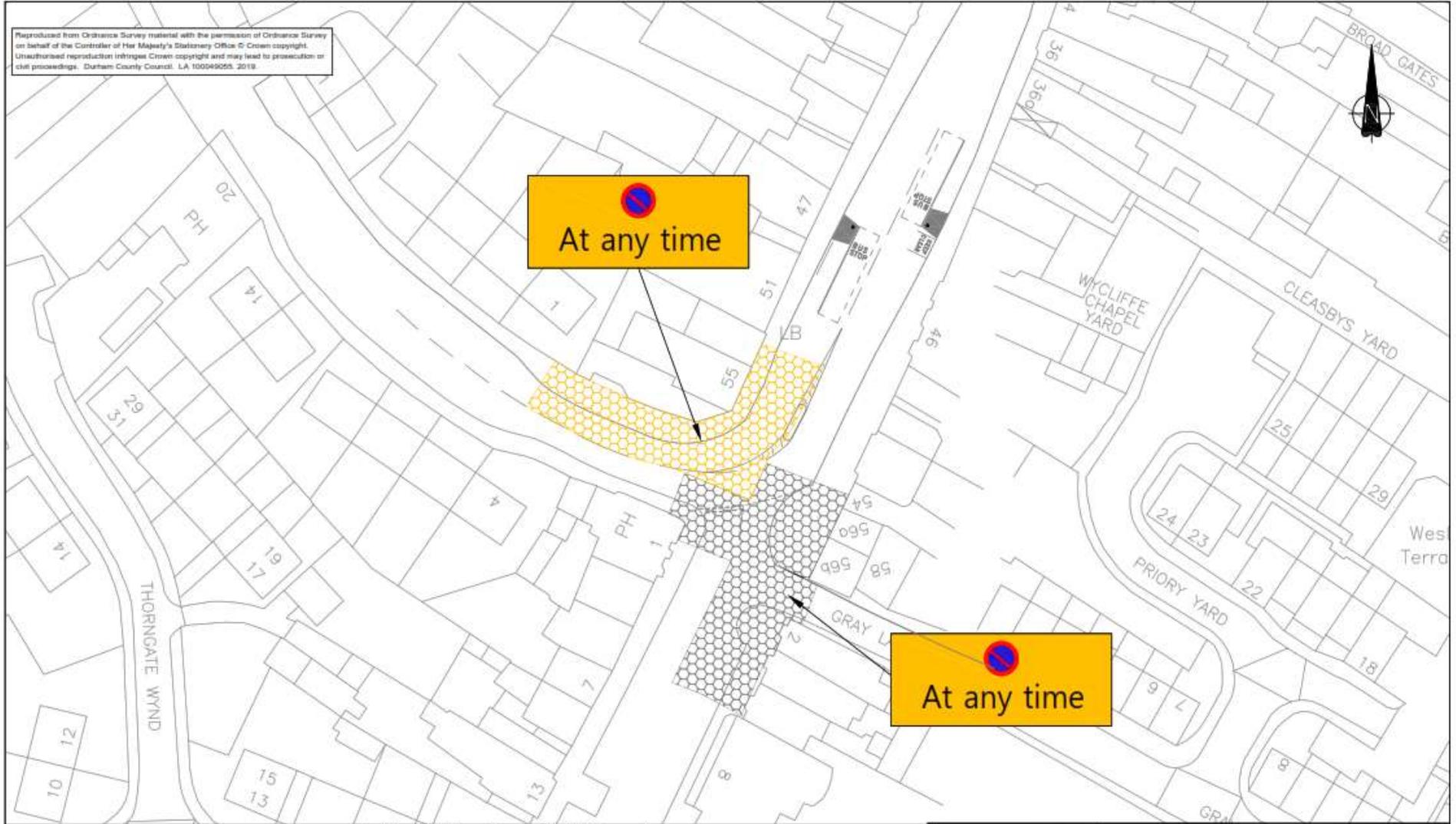
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Project/Drawing No. CE43

Rev.

Proposal– Bridgegate/ The Bank No Objections

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	Proposed No Waiting At Any Time
	Existing No Waiting At Any Time
	Existing Restricted Waiting

Rev/ Amdt	Drawn By		Approved By			Description Of Amendments
	Name	Date	Name	Date	Sign	
	P. Broxton	June 19				

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	Rev.

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